



Northumberland

County Council

NORTH NORTHUMBERLAND LOCAL AREA COUNCIL
RIGHTS OF WAY SUB-COMMITTEE
13 February 2019

REVIEW OF THE DEFINITIVE MAP AND STATEMENT OF PUBLIC RIGHTS OF WAY

ALLEGED BYWAY OPEN TO ALL TRAFFIC No 1 NEWTON-BY-THE-SEA (part 1)

Report of the Executive Director of Local Services
Cabinet Member: Councillor Glen Sanderson, Environment and Local Services

Purpose of report

In this report, the Council is asked to consider all the relevant evidence gathered in support and in rebuttal of the existence of public vehicular rights over a short length of the U3072 road, which is also recorded as being part of Public Footpath No 1, in the vicinity of Newton Link House.

Recommendation

It is recommended that the Council agrees that:

- (i) there is sufficient evidence to indicate, on a balance of probabilities, that public vehicular rights exist over the route K-L;**
- (ii) there is sufficient evidence to indicate, on a balance of probabilities, that public bridleway rights exist over the route J-K, and that this section be included in a future Definitive Map Modification Order to upgrade this short length of public footpath to public bridleway;**
- (iii) the Natural Environment and Rural Communities Act 2006 would not appear to have extinguished the public's motorized vehicular rights over the K-L route;**
- (iv) the K-L section be included in a future Definitive Map Modification Order upgrading this short length of public footpath to byway open to all traffic status.**

1.0 BACKGROUND

1.1 By virtue of section 53 of the Wildlife and Countryside Act, 1981 the County Council is required to keep the Definitive Map and Statement under continuous review and make modification orders upon the discovery of evidence, which shows that the map and statement need to be modified.

1.2 The relevant statutory provision which applies to adding a byway open to all traffic to the Definitive Map and Statement based on historical documentary evidence is Section 53(3)(c)(i) of the Wildlife and Countryside Act, 1981, which require the County Council (as Surveying Authority) to modify the Definitive Map and Statement following:

“the discovery by the authority of evidence which (when considered with all other relevant evidence available to them) shows:

“that a right of way which is not shown in the map and statement subsists or is reasonably alleged to subsist over land in the area to which the map relates, being a right of way such that the land over which the right subsists is a public path, a restricted byway or, subject to section 54A, a byway open to all traffic;”

1.3 Alternatively, in a case where the route is already shown on the Definitive Map as a highway of a lesser status than Section 53(3)(c)(ii) covers situations where:

“the discovery by the authority of evidence which (when considered with all other relevant evidence available to them) shows:

“that a highway shown in the map and statement as a highway of a particular description ought to be there shown as a highway of a different description;”

1.4 All the relevant statutory provisions and competing rights and interests have been considered in making this report. The recommendation is in accordance with the law and proportionate, having regard to individuals' rights and the public interest.

2.0 PUBLIC EVIDENCE

2.1 The route of what is now the U3072 road, to Newton Link House, has appeared as a road or track on historical maps from at least 1828 onwards. The route may be somewhat crudely shown on the 1932 Handover Map and is identified on the map produced in the late 1930s relating to the Restriction of Ribbon Development Act 1935. It appears on the Council's 1951 Highways Map and on all the known Council highways maps and schedules produced subsequently. It was shown on the Council's List of Streets that existed as at 2 May 2006. The earlier highway maps are generally small scale, making it difficult to determine, with any great precision, exactly where the northern end of the U3072 is. The 25" Highways Map and, indeed, the present day List of Streets both depict the road as terminating at Point K. The List of Streets, as at 2 May 2006, erroneously showed the road continuing well beyond Point K.

- 2.2 The Definitive Map of Public Rights of Way for Northumberland was originally published in 1962. It was the culmination of a process that involved the production of an informal "Survey" map, followed by the formal publication of "Draft" and "Provisional" Maps which were open to public scrutiny and challenge. The section of Map covering the Alnwick Rural District Council area had a Relevant Date of 20 September 1954. A County wide "First Review" of the Definitive Map was completed in the early 1970s. This review had a Relevant Date of 1 November 1963 (i.e. it took account of all know changes which occurred before November 1963). Where no change to a public right of way had taken place, the First Review Definitive Map should show that path on the same alignment and with the same status as the preceding, original Definitive Map. The Wildlife and Countryside Act 1981 changed the way that the Definitive Map was to be kept up-to-date. Instead of producing new maps showing every public right of way, Definitive Map Modification Orders would be made to add, remove or alter individual paths. At any given moment in time, the 'official' Definitive Map would henceforth be the last published map, taken in conjunction with all the 'confirmed' Definitive Map Modification Orders which amended it. By 2005, the First Review Definitive Map had become extremely out-of-date (i.e. many hundreds of paths shown on it had been stopped up or diverted and an even greater number of 'new' paths had been added). To remedy this, that year the Council formally consolidated its Definitive Map. This process involved publishing a new Definitive Map (Relevant Date of 1 December 2005) that took account of all the Definitive Map Modification Orders which had been made and confirmed, up to that point. Again, where no change to public rights of way had taken place, the consolidated Definitive Map should show paths on the same alignment and with the same status as the preceding, First Review Definitive Map.
- 2.3 On the original Survey, Draft and Provisional Maps the public rights of way proceeding north-westwards and south-eastwards from Newton Link House are clearly shown and numbered as two separate paths (numbered "1" and "2"). On the original Definitive Map, they are still shown as being two separate paths but now, somewhat unhelpfully, both sections are identified as being parts of Public Footpath No 1. On the First Review Definitive Map, the two paths are still shown as being physically separated by a short length of road, with both sections being numbered as parts of Footpath No 1. On the consolidated Definitive Map, Footpath No 1 is now shown as a single continuous route. It isn't known whether the path was shown extending along the K-L section of the U3072 road by accident or whether this was done deliberately. Footpath No 1 hadn't been been realigned by any legal process, so the consolidated map should have reflected what was on the preceding First Review Map. What is known is that the GIS program that was in use at the time all the rights of way information was being digitized, during the late 1990s and early 2000s, could not accommodate situations where there was more than one path, in any given parish, with the same number. Where these situations did occur, the GIS program was known to automatically draw a line to connect the two paths. It is also possible that the person undertaking the digitizing deliberately drew the various separate parts of Footpath No 1 as a continuous line because they knew that, even if they didn't, the GIS program would join them together (potentially in a more random fashion) anyway.

- 2.4 On the original Survey, Draft and Provisional Maps the public footpath proceeding north-westwards and public bridleway proceeding north-eastwards from Newton Link House are shown beginning at Point J. On the original Definitive Map, these two paths appear to be shown beginning, slightly further south, at Point K. On the First Review Definitive Map, the two paths are once again shown beginning at Point J. On the consolidated Definitive Map (i.e. the current Map) Footpath No 1 is now shown as a single continuous route, passing through points J-K-L, and Bridleway No 2 is shown beginning at Point J. The distance between Point J and Point K is only about 10 metres (i.e. 1 millimetre on a 1:10,000 scale map), but if it is agreed that Point K represents the northern end of the public road and Point J reflects the existing southern extent of Public Bridleway No 2, then there exists a cul-de-sac public bridleway that's only connection to the rest of the highway network is via a short length of public footpath.
- 2.5 In conclusion, currently, the potential for confusion exists because, at Newton Link House, the same 40 metre long stretch of road is simultaneously identified on the Council's 'List of Streets' as being part of the U3072 road and on the Definitive Map of Public Rights of Way as being part of Public Footpath No 1. Furthermore, at present Public Bridleway No 2 begins at Point J and proceeds north-eastwards. If the northern end of the road is accepted to be Point K, then there would be no lawful means for equestrian (or bicycle) traffic to access the bridleway, unless the short K-J section of existing Footpath No 1 was determined to have bridleway status.
- 2.6 The Definitive Map and Statement of Public Rights of Way are legal documents which identify the existence of public highway rights (footpaths, bridleways, restricted byway and byways open to all traffic), but the Definitive Map and Statement do not usually identify who has the maintenance responsibility for those rights of way. The List of Streets is a document which identifies which routes the County Council acknowledges it has a maintenance responsibility for, but a route's inclusion on that list does not establish its status (i.e. it need not necessarily be a vehicular rights of way).
- 2.7 Whilst there is no obstacle to a publicly maintainable public footpath being identified on the Council's List of Streets, it is not considered that this 40 metre long section of public highway was recorded on that basis, or that the rest of the U3072 is in fact no more than a public footpath. This report will consider the available evidence to determine what the correct status of the J-K-L section of existing Public Footpath No 1 is, and explore whether or not it is appropriate for this section of footpath to be upgraded so as to accurately reflect any higher rights which might exist.
- 2.8 The 1974 landowner highway dedication appears to have been made to facilitate the proposed public car park, north of Point J (see text annotation on the dedication plan). Notwithstanding this, by 1979, records show that Alnwick District Council had acquired the rectangle of land east of Point K, where the present day Newton Steads car park sits. The acquisition of this land / provision of a car park does suggest that this area was popular with the visiting public, that public parking was perhaps creating difficulties, and that the provision of a public car park was Alnwick District Council's way of addressing that problem.

- 2.9 As can be seen from the 1:25,000 scale OS Explorer map extract (reproduced here at 1:10,000 scale) the route of Public Footpath No 1, including the short section that is also identified as being part of the U3072 road at Newton Link House, is part of St Oswald's Way; a popular (97 mile long) long distance trail between Hexham and Holy Island. The route, which was opened in 2006, uses existing public rights of way and minor public roads. The same section of Footpath No 1 is also part of the Northumberland Coast Path, which is understood to have been designated in 2005.

3. LANDOWNER EVIDENCE

- 3.1 In July 2014, the County Council consulted regarding a proposal to delete the short section of Public Footpath No 1 (from point K to point L) from the Definitive Map on the basis that public motor vehicular rights exist over this short section of the footpath. No owners or occupiers of the land affected replied to this consultation.

4. CONSULTATION

- 4.1 In July 2014, the Council carried out a consultation with the Parish Council, known owners and occupiers of the land, the local County Councillor and the local representatives of the "prescribed and local organisations" listed in the Council's "Code of Practice on Consultation for Public Path Orders". Two replies were received and are included below.

- 4.2 By letter, dated 28th October 2014, Mr D Roberts responded to the consultation, on behalf of the Cyclists' Touring Club, stating:

"I attach comments and evidence which I judge to be relevant on behalf of Cyclists Touring Club.

"To clarify locations I attach annotated copies of your plans. These are confined to routes of which I have knowledge. Where I have no knowledge of a route or where I know it is surfaced with tarmac and therefore suitable for ALL TRAFFIC I have omitted any comment.

"Where an obstruction has been encountered I have endeavoured to provide as accurate date as possible.

"Routes traced in red have been cycled without problem. [The J-K-L section of existing Footpath No 1 was not one of the ones highlighted by Mr Roberts].

"I have commented on issues where I consider an omission has been made in your consultation.

"No distinction has been made between Restricted and All Traffic Byways."

- 4.3 By email, on 11th November 2014, Ms S Rogers responded to the consultation, on behalf of the British Horse Society, stating:

“NEWTON-BY-THE-SEA PARISH

Footpath 1 deletions

The path sections shown on maps 15 & 57 run along tarmac roads. As rights for walkers already exist along them, there is no need for them to be shown on the definitive map as public footpaths. So the BHS supports these deletions.”

5. DOCUMENTARY EVIDENCE

- 5.1 A search has been made of archives relating to the area. Evidence of Quarter Sessions Records, Council Highways records, County Maps and O.S. Maps was inspected, and the following copies are enclosed for consideration.

1769 Armstrong's County Map

There is some evidence of a road or track approximating to the U3072 road with a continuation along the coast, beyond Point K / Newton Link House.

1820 Fryer's County Map

There is no evidence of a road or track approximating to the U3072 road.

1820-32 Cary's Map

There is no evidence of a road or track approximating to the U3072 road.

1828 Greenwood's County Map

There is evidence of a road or track approximating to the U3072 road with a continuation along the coast, beyond Point K / Newton Link House.

c.1860 Ordnance Survey Map: Scale 1:2500

There is clear evidence of an enclosed road over the northern end of the U3072 road. This road (including the Point K - Point L section) is identified with the parcel number “38” on the map, and in the accompanying Book of Reference the corresponding entry identifies this parcel as “Public road”. A subordinate path / track is shown joining the road from the south-east, at Point L. Two unenclosed paths / tracks continue north-westwards and northwards, beyond Point K, over Newton Links.

1897 Ordnance Survey Map: Scale 1:2500

There is clear evidence of an enclosed road over the northern end of the U3072 road. A subordinate path / track (labelled “FP”) is shown joining the road from the south-east, at Point L. Two mainly unenclosed (they are both, briefly, enclosed) paths / tracks continue

north-westwards (labelled “FP”) and northwards, beyond Point K, over Newton Links.

1923 Ordnance Survey Map: Scale 1:2500

There is clear evidence of an enclosed road over the northern end of the U3072 road. A subordinate path / track (labelled “FP”) is shown joining the road from the south-east, at Point L. Two mainly unenclosed (they are both, briefly, enclosed) paths / tracks continue north-westwards and northwards, beyond Point K, over Newton Links.

1932 Alnwick Rural District Council Handover Map

A publicly maintainable highway over the route of U3072 route is suggested by a route, marked in pencil, which extends into the border of the map sheet (because the route would actually lie on the map sheet immediately to the north) though this could be intended to show no more than the middle section of the C72 road. Certainly, given this somewhat crude indication of the route, and the small scale of the mapping, it isn't possible to determine whether or not the K-L section is identified as part of the maintainable road being handed over.

c.1938 Restriction of Ribbon Development Act 1935 Map

The route of the U3072 road is clearly coloured / identified as a public highway (numbered “132”) to be protected from ribbon development. Although the base map is relatively small scale (one inch to the mile) it would appear that the K-L section is depicted as being part of this road.

1951 Highways Map

The route of the U3072 road is coloured in purple so as to identify it as a publicly maintainable road. Although the base map is relatively small scale, the K-L section does appear to be depicted as being part of this road.

c.1951 Definitive Map – original Survey Schedules & Map

The route of the U3072 road exists on the base map and has been coloured brown. Known public roads were generally coloured brown to indicate what the extent of the road network was considered to be. It is not identified as a public right of way which should be included on the Definitive Map. The subordinate route, joining from the east, at Point L, is identified as a public bridleway (“2”) and the tracks proceeding north-westerly and northerly from Point J are also identified as public bridleways (“1” and “10”).

Draft Map

The route of the U3072 road exists on the base map but it is not identified as being either a public footpath, public bridleway or Road Used as a Public Path (RUPP). A pencilled note on the Map indicates that at Point K there is a “Private Road Sign”. The subordinate route, joining from the east, at Point L, is now identified as a public footpath

(still "2") and the tracks proceeding north-westerly and northerly from Point J are now identified as a public footpath (still "1") and public bridleway (still "10"), respectively.

Provisional Map

The route of the U3072 road exists on the base map but it is not identified as being either a public footpath, public bridleway or Road Used as a Public Path (RUPP). The subordinate route, joining from the east, at Point L, is identified as a public footpath ("2") and the tracks proceeding north-westerly and northerly from Point J are identified as a public footpath ("1") and public bridleway ("10").

1957 Ordnance Survey Map: Scale 1:10,560

There is clear evidence of an enclosed road / track over the route of the U3072 road. A subordinate path / track (labelled "FP") is shown joining the road from the south-east, at Point L. Two mainly unenclosed (they are both, briefly, enclosed) paths / tracks continue north-westwards (labelled "FP") and northwards, beyond Point K, over Newton Links.

Original Definitive Map

The route of the U3072 road exists on the base map but is not identified as being either a public footpath, public bridleway or Road Used as a Public Path (RUPP). The subordinate route, joining from the east, at Point L, is identified as a public footpath (now part of "1") and the tracks proceeding north-westerly and northerly from Point K (rather than Point J) are identified as a public footpath (part of "1") and public bridleway (now "2").

First Review Definitive Map (Relevant date 1 Nov 1963)

The route of the U3072 road exists on the base map but is not identified as being either a public footpath, public bridleway or Road Used as a Public Path (RUPP). The subordinate route, joining from the east, at Point L, is identified as a public footpath (part of "1") and the tracks proceeding north-westerly and northerly from Point J are identified as a public footpath (part of "1") and public bridleway ("2").

1964 Highways Map

The route of the U3072 road is coloured in purple, so as to identify it as a publicly maintainable road. Although the base map is relatively small scale, the K-L section appears to be depicted as being part of this road.

c.1970 - 1990s Highways Map 6"

The route of the U3072 road is coloured in purple, so as to identify it as a publicly maintainable road. Whilst the K-L section appears to be depicted as being part of this road, the J-K section would seem not to.

c.1970s - 1990s Highways Map 25"

The route of the U3072 road is coloured in purple, so as to identify it as a publicly maintainable road. The K-L section is clearly depicted as being part of this road, and the J-K section is clearly identified as not being part of this road.

1974 Highway dedication

There is clear evidence that on 16 February 1974 the landowner dedicated a triangular section of land widening the U3072 road between Points K and L.

1978 Ordnance Survey Map: Scale 1:10,000

There is clear evidence of an enclosed road / track over the route of the U3072 road. A subordinate path / track (labelled "Path") is shown joining the road from the south-east, at Point L. Two mainly unenclosed (they are both, briefly, enclosed) paths / tracks continue north-westwards (labelled "Path") and northwards, beyond Point K, over Newton Links.

2005 Consolidated Definitive Map Scale 1:10,000

There is clear evidence of an enclosed road / track over the route of the U3072 road shown on the OS base map used. The route J-K-L is clearly identified as being part of Public Footpath No 1. Public Bridleway No 2 begins at Point J and proceeds in a general north-easterly direction towards the coast.

2006 The Council's 'List of Streets' (2 May 2006)

The route of the U3072 road is clearly identified as publicly maintainable highway. The road is actually shown (mistakenly) with a 45 metre long spur extending in a north-westerly direction from its northern end at Point K.

5.2 The route of the alleged byway is not identified in the Schedule of unclassified roads identified under the provisions contained within the Restriction of Ribbon Development Act 1935, despite being clearly identified as Road 132 on the Map. Road 131 is the final entry in the schedule. This suggests that either this road was omitted by some oversight or, alternatively, that it was included as a later addition.

5.3 The entry for the U3072 road, in the 1958 County Road Schedule is:

"U3072 Links Road Newton
From C72 at Newton by the Sea to Newton Links House."

The length of the U3072 road is identified as being 0.57 miles.

5.4 The entry for the U3072 road, in the 1964 County Road Schedule is:

“U3072 Newton Links Road
From C72 at Newton-by-the-Sea northwards to Newton Links House.”

The length of the U3072 road is identified as being 0.57 miles.

5.5 The entry for the U3072 road, in the 1974 County Road Schedule is:

“U3072 Newton Links Road
From C72 at Newton-by-the-Sea (NU 235252) northwards to Newton Links House (NU 235260).”

The length of the U3072 road is identified as being 0.57 miles.

5.6 The original Definitive Statements for the public rights of way that connected with the northern end of the U3072 road state:

Public Footpath No 1 (Newton-by-the-Sea)

“From FP 5 at the Beadnell Parish boundary at the Suspension Bridge over Long Nanny in a South-easterly and southerly direction by the public road at Newton Links House and crossing the road at Newton Seahouses to join FP 1 at the Embleton Parish boundary.”

Public Bridleway No 2 (Newton-by-the-Sea)

“From the public road at Newton Links House in a northerly direction across Newton Links to the Sea.”

6. SITE INVESTIGATION

6.1 From a point marked L, 65 metres south-east of Link House Farm, where Public Footpath No 1 (proceeding northwards from Low Newton) joins the U3072 road, a 3 to 3.5 metre wide tarmac road (with double yellow lines down both sides), within an 8 to 12 metre wide corridor, proceeds in a north-easterly direction for a distance of 35 metres, passing across the entrance to the Council’s Newton Steads Car Park, as it does so. The road then continues in a northerly direction for a distance of 5 metres to a point marked K. At point K there is signage advertising Link House Farm Self Catering Holiday Cottages, and also stating “PRIVATE Residents and Holiday Guests Only” with regard to the north-westerly continuation towards Link House. Another, part tarmac, part stone surfaced track continues in a north-easterly direction for a distance of 10 metres, through a field gate to a point marked J (the existing junction of Public Footpath No 1 and Public Bridleway No 2), 65 metres east of Link House Farm.

7. DISCUSSION

7.1 Section 53 (3)(c) of the Wildlife and Countryside Act 1981, requires the County Council to modify the Definitive Map when evidence is discovered which, when considered with all other relevant evidence available to them shows:

that a right of way, which is not shown in the Map and Statement, subsists or is reasonably alleged to subsist over land in the area to which the Map relates, being a right of way such that the land over which the right subsists is a public path, a restricted byway or; subject to section 54A, a byway open to all traffic; or
that a highway shown in the map and statement as a highway of a particular description ought to be there shown as a highway of a different description; or
that there is no public right of way over the land shown in the map and statement as a highway of any description, or any other particulars contained in the map and statement require modification.

- 7.2 When considering an application / proposal for a modification order Section 32 of the Highways Act, 1980 provides for “any map, plan or history of the locality or other relevant document” to be tendered in evidence and such weight to be given to it as considered justified by the circumstances, including the antiquity of the tendered document, the status of the person by whom and the purpose for which it was made or compiled, and the custody in which it has been kept and from which it is produced.
- 7.3 The representation of a path or track on an Ordnance Survey Map is not evidence that it is a public right of way. It is only indicative of its physical existence at the time of the survey.
- 7.4 It isn't entirely clear when the U3072 road was first identified as a publicly maintainable highway. Its existence is suggested by the pencil markings on the 1932 Handover Map, but this might simply be a clumsy representation of the C72 road. It is depicted as Road 132 on the map produced in relation to the Restriction of Ribbon Development Act 1935, but there is no corresponding entry for that road in the published schedule. The road is unambiguously identified as the U3072 on 1951 Highways map and is identified as a public road on the Survey Map produced in c.1951 as a precursor to preparation of the first Definitive Map. The route has been consistently identified as a publicly maintainable highway on all the Council maps and schedules produced from then, onwards.
- 7.5 If the K-L section of Footpath No 1 had been identified as Part of Public Footpath No 1 on the original Definitive Map when it was being drawn up in the 1950s this would have been a positive indication that this section was considered to be no more than a public footpath, but it wasn't, nor was it shown as a public footpath on the First Review Map, which indicates there had been no discovery of evidence to suggest the route is no more than a public footpath. No Definitive Map Modification Orders were made under the Wildlife and Countryside Act 1981 identifying the K-L section as a public footpath, therefore the consolidated Definitive Map should simply have shown Footpath No 1 in the same way as the First Review Map. As discussed previously, it is not clear why Footpath No 1 is shown over the K-L section. The original and First Review Definitive Maps showed Footpath No 1 as a single public right of way, split into 3 parts. The present situation may not have arisen if the 3 parts had been numbered as 3 separate paths, as originally intended, and depicted, on the Survey, Draft and Provisional Maps. However, for some reason, between the Provisional Map and Definitive Map stages, the decision was made to re-number the 3 separate parts as one single path (numbered “1”).

- 7.6 It may have seemed a logical step, identifying this coastal route with a single path number, and it probably didn't cause any issues at the time, but it was unnecessary. Ultimately, it also proved to be decidedly unhelpful, as the geographic information system (GIS) that was used to map the whole rights of way network in the late 1990s / early 2000s could not handle having more than one path in a parish with the same number. It coped with this by adding its own lines to join path sections together. This situation didn't occur very often because, usually, separate sections of path were given their own unique numbers. The situation might arise where the middle section of a path was diverted or stopped up to facilitate, say, a housing or commercial development. The GIS would then draw a single straight line to marry the two parts together, regardless of what physical terrain or buildings might lie in between. To get around this shortcoming, prior to consolidation, we made a number of legal Orders formally renumbering many sections of path. This path seems to have been overlooked during that exercise, possibly because the connecting lines didn't stand out.
- 7.7 The route of the U3072 has been consistently identified as an enclosed road / track on Ordnance Survey maps since c.1860. Although it is not shown on Fryer's Map of 1820 or Cary's Map of 1820-32, it is clearly shown on Greenwood's County Map of 1828 and might also be depicted on Armstrong's County Map of 1769.
- 7.8 The County Council accepts that, given the way the regulations were written with regard to the way highway authorities could include publicly maintainable highways in the List of Streets, there was no impediment to public bridleways and public footpaths also being included. That is not to say that any bridleways or footpaths were so shown – just that they could be. It must, therefore, be entirely proper to consider each UCR on a case by case basis, but that does not mean that we should begin with the assumption that each UCR is no more than a public footpath unless higher rights can be proven by other means. In Northumberland there is no evidence to suggest that public footpaths and public bridleways were deliberately shown on the 1958, 1964 or 1974 County Road Schedules (forerunners of the modern day List of Streets). The fact that a route is shown on these schedules must, therefore, be evidence of some weight that public vehicular rights exist.
- 7.9 A separate though related matter concerns the correct status of the J-K section of existing Public Bridleway No 1. As discussed in paragraph 2.4 above, the southern end of existing Public Bridleway No 2 does not connect with the northern end of the U3072 road at Point K; instead it terminates 10 metres north of this, on existing Footpath No 1, at Point J. If correct, this would mean that horse riders and cyclists had no legal right to access the bridleway (i.e. they'd have to trespass on either the 10 metre stretch of public footpath (J-K) or on some even longer alternative route). Although the Survey, Draft and Provisional Maps all showed the bridleway (and footpath) ending at Point J, the original Definitive Map showed it extending to Point K. The First Review and consolidated Maps then both had the bridleway terminating back at Point J. Perhaps most significantly, though, the Definitive Statement,

produced to accompany the original Definitive Map (and unchanged by the First Review / consolidation Definitive Map) describes the route of the public bridleway as "From the public road at Newton Links House". Where the Definitive Map and Definitive Statement conflict, the two need to be considered together to determine what the correct position is - it is not the case that the Map automatically takes precedence over the Statement, or vice versa. The position, as currently shown on the Definitive Map, whilst not completely impossible, is considered to be highly improbable. Given the short distance involved and the scale of the mapping, some minor confusion regarding the location of the northern end of the road and / or a drafting error would seem to be the most likely explanation. Thus, on a balance of probabilities, it would seem reasonable to conclude that Public Bridleway No 2 did (and still does) connect with the public road. If it is accepted that the evidence indicates that the public road ended at Point K, then Public Bridleway No 2 must extend southwards over the route J-K to meet it.

- 7.10 Letters from DEFRA, dated 2003 and November 2006, and Rights of Way Circular 1/09 set out the approach Inspectors and order making authorities should take in determining the status of routes included on the List of Streets. In summary, the guidance states that the inclusion of a route on the List of Streets is not a record of what legal rights exist over that highway but may provide evidence of vehicular rights. However, this must be considered with all other relevant evidence in order to determine the nature and extent of those rights. Highway Authorities are recommended to examine the history of such routes and the rights that may exist over them on a case by case basis in order to determine their status.
- 7.11 The Natural Environment and Rural Communities Act 2006 (NERC Act 2006) had a major impact upon the recording of byways open to all traffic based upon historical documentary evidence. Under section 67 of the Act, any existing, but unrecorded, public rights of way for mechanically propelled vehicles were extinguished unless one of the 'saving' provisions applied. In brief, these saving provisions were: (a) if the main lawful public use between 2001 and 2006 was with motor vehicles; (b) if the route was on the List of Streets (on 2 May 2006) and not also on the Definitive Map as something less than a byway open to all traffic; (c) the route was legally created expressly for motor vehicular use; (d) the route was a road deliberately constructed for public motor vehicular use; or (e) the vehicular highway came about as a result of unchallenged motor vehicular use before December 1930.
- 7.12 Of the saving provisions above, (b) will clearly not apply to the K-L section of the U3072 road. Where a route is shown on the Definitive Map as a footpath or bridleway, then the fact that it is shown on the List of Streets will not be sufficient to prevent the public's motor vehicular rights from being extinguished. It is necessary to see whether or not one of the other saving provisions might apply.

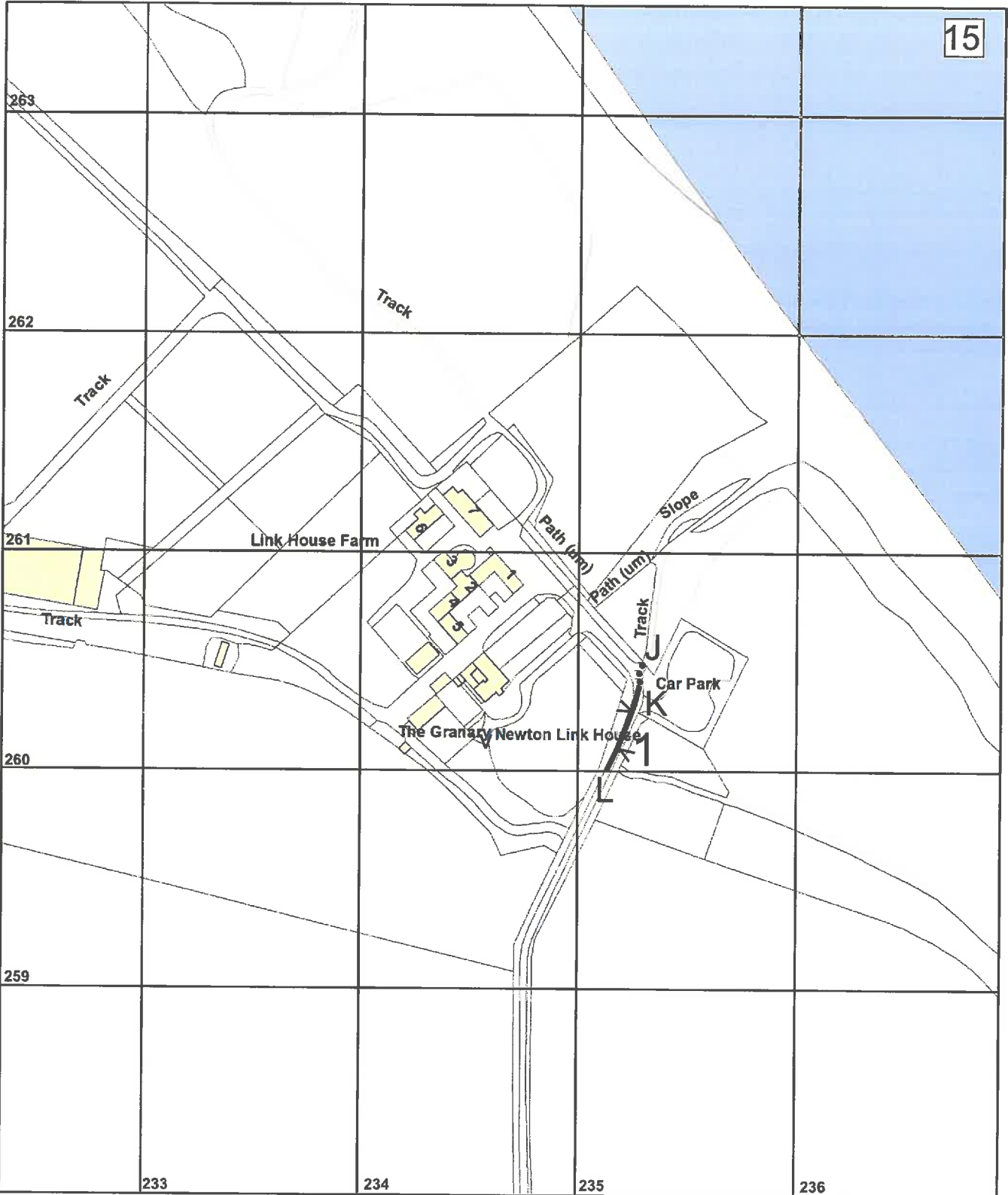
- 7.13 Under section 67(2)(c) of NERCA 2006, the public's motor vehicular rights would not be extinguished if those rights had been created on terms that expressly provided for a right of way for mechanically propelled vehicles. Officers are not aware of any documents associated with dedicating the U3072, as a whole, as a public road. There is a 1974 highway dedication, expanding the width of the K-L section, but that dedication is of "a highway". Whilst the intention was, almost certainly, to dedicate a "vehicular" public right of way, it doesn't "expressly" say so. Since public footpaths and public bridleways are also highways, this particular dedication is not considered to satisfy the requirements of this section of the Act.
- 7.14 Under section 67(2)(a) of NERCA 2006, the public's motor vehicular rights would not be extinguished if they are over a way whose main lawful use by the public during the period of 5 years between 2nd May 2001 and 2nd May 2006 was with mechanically propelled vehicles. The difficulty here is that the Council has no evidence regarding the balance of public user during this 5 year period. That said, the U3072 is a well made up tarmac road which (although a vehicular cul-de-sac) is clearly used on an everyday basis by the public with 'normal' motor vehicles. The main type of use was likely to be local traffic, gaining access to the various residential / rental properties at Link House Farm and also people accessing the public Newton Steads Car Park. This car park is popular, particularly in the summer. Although some people may have parked their cars there, then walked along the road, it is believed that the majority of people probably used the exits at the north and south-east of the car park, in order to access the beach and the existing path network in that area. On a balance of probabilities, therefore, it is believed that this balance of user saving provision will apply to the J-K section of U3072 road.
- 7.15 Under section 67(2)(e) of NERCA 2006, the public's motor vehicular rights would not be extinguished if they are over a way that had been in long use by mechanically propelled vehicles before 1930, when it first became an offence to drive 'off-road'. There is no evidence of long-standing public motor vehicular use before 1930 in this instance.
- 7.16 Finally, under section 67(2)(d) of NERCA 2006, the public's motor vehicular rights would not be extinguished if they are over a way that had been created by construction of a road intended to be used by MPVs. The modern day tarmac road was clearly constructed with motor vehicular use in mind but, based upon the historical documentary evidence available, it was almost certainly a public road before a tarmac surface was applied. On that basis this saving provision will not apply.
- 7.17 The U3072 road has a decent tarmac surface and is in everyday use by the public with motor vehicles. Based on the historical documentary evidence available, and the pattern of public user it seems clear that the route is a public vehicular right of way. As discussed in 7.14 above, it is believed that during the period May 2001 to May 2006, public motor vehicular use of the K-L

section of the route (especially between Point L and the car park entrance) exceeded the combined total of pedestrian, equestrian and bicycle use, thereby 'saving' the public's motor vehicular rights from being extinguished by the NERC Act 2006.

- 7.18 For a route to be a byway open to all traffic, it has to be (i) a public motor vehicular right of way and (ii) a route which is nevertheless used (or is likely to be used) by the public mainly for the reasons which footpaths and bridleways are used.
- 7.19 In 2005, the Northumberland Coast Path was designated and, in 2006, St Oswald's Way long distance walking trail was launched. Footpath No 1 forms part of both these route. Officers are not suggesting that Footpath No 1 was not well walked before, but it is certainly the case that the creation of these two trails led to a significant increase in pedestrian use of the J-K-L route. The trails were advertised in guidebooks and with leaflets, promoted with new signposting and waymarking, and use was facilitated by infrastructure improvements and vegetation clearance on the ground. It is considered that, in the present day, pedestrian use of the route (combined with any limited equestrian and bicycle use) will exceed public vehicular use, thereby satisfying the criteria for being recorded as a byway open to all traffic.
- 7.20 Advice from the Planning Inspectorate in their 'consistency guidelines' states that it is important to have the correct width, where known, recorded in the definitive statement. Usually there is a boundary to boundary presumption for public highways. On this basis, if an Order were to be made to record the K-L section as a byway open to all traffic or restricted byway, it would be with a width of 8 to 12 metres.

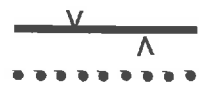
8. CONCLUSION

- 8.1 In light of the user and documentary evidence submitted, it appears that, on a balance of probability, public vehicular rights have been shown to exist over the K-L section of existing Public Footpath No 1, and public bridleway rights have been shown to exist over the J-K section of existing Public Footpath No 1.
- 8.2 The Natural Environment and Rural Communities Act 2006 would not appear to have extinguished the public's motor vehicular rights over the K-L part of the U3072 road.
- 8.3 Notwithstanding, that the reason for arguing that the public's motor vehicular rights had not been extinguished by the Natural Environment and Rural Communities Act 2006 (see 7.17 above), for the reason in paragraph 7.19 above, it would be appropriate to recognize the public's motor vehicular rights over the K-L section of Footpath No 1 by upgrading it to a byway open to all traffic.




Northumberland
 County Council
 Infrastructure
 Local Services Group
 County Hall Morpeth Northumberland
 Telephone 0845 600 6400 NE61 2EF

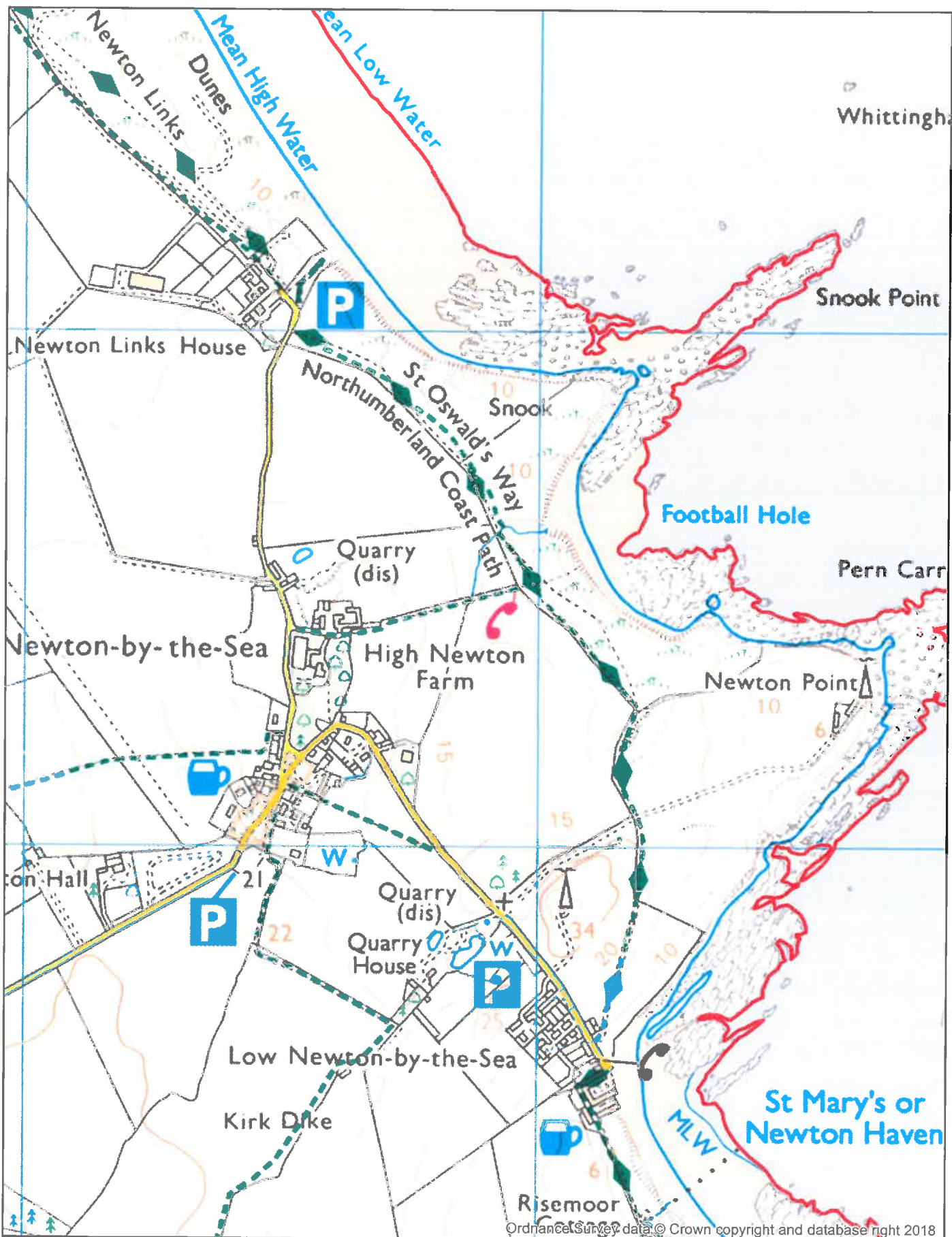
Wildlife and Countryside Act 1981 Public Rights of Way



Alleged Byway Open to All Traffic
 Public Footpath to be upgraded to Public Bridleway

This map is reproduced from Ordnance Survey material with the permission of Ordnance Survey on behalf of the Controller of H.M.S.O. © Crown Copyright. Unauthorised reproduction infringes Crown Copyright and may lead to prosecution or civil proceedings. Northumberland County Council O.S. Licence No. 100049048 (2012)

Former District(s) Alnwick	Parish(es) Newton by the Sea	Scale 1:2500
Def. Map No. 42	O.S. Map NU 22 NW	Date January 2019



Ordnance Survey data © Crown copyright and database right 2018

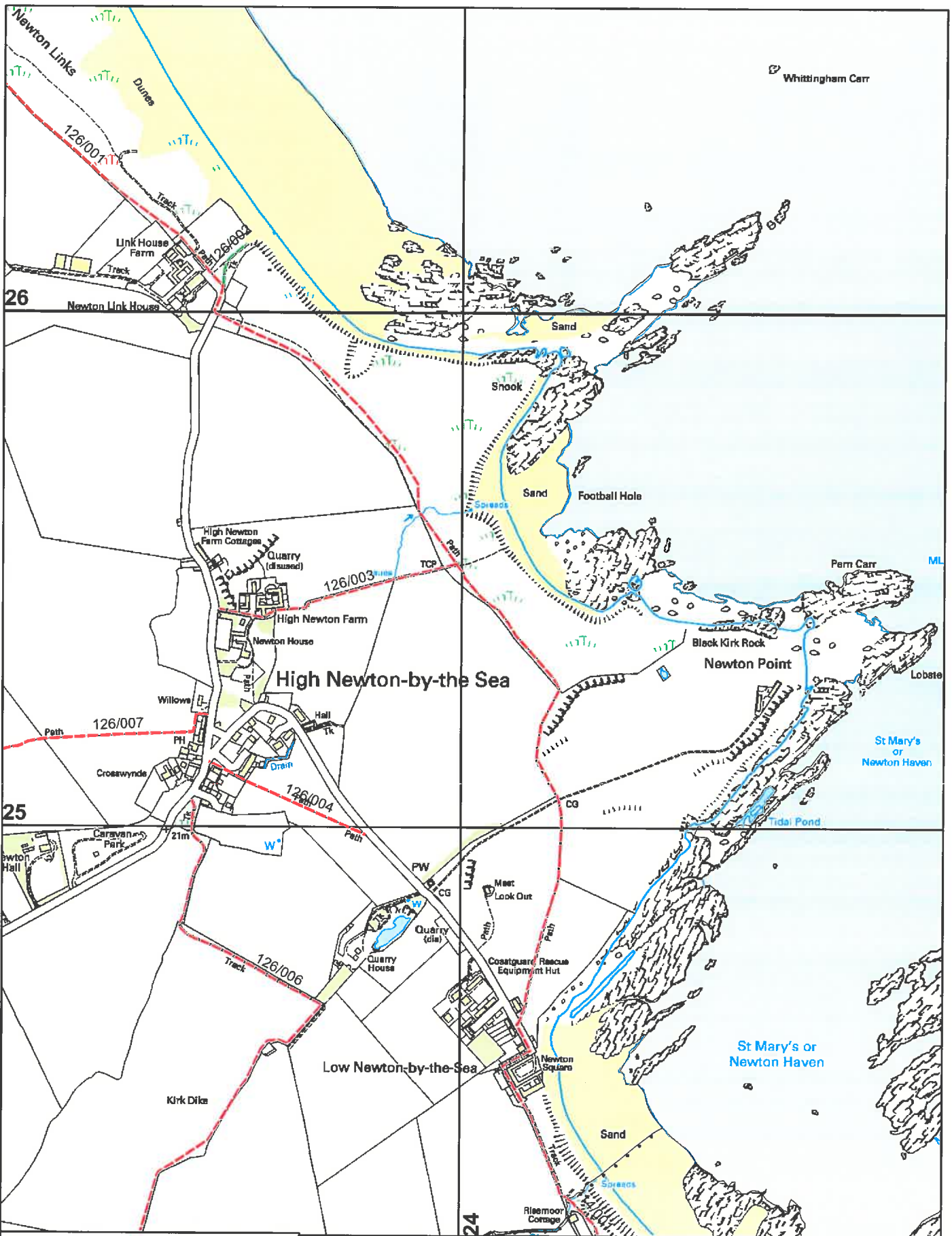


Northumberland
County Council

Contact: Alex Bell
Telephone: 01670 624133
Email: Alex.Bell@northumberland.gov.uk

Location Map

This map is reproduced from Ordnance Survey material with the permission of Ordnance Survey on behalf of the Controller of Her Majesty's Stationary Office Crown Copyright. Unauthorised reproduction infringes Crown Copyright and may lead to prosecution or civil proceedings. License no. 100049048 (2010).



Northumberland
County Council

Contact: Alex Bell
Telephone: 01670 624133
Email: Alex.Bell@northumberland.gov.uk

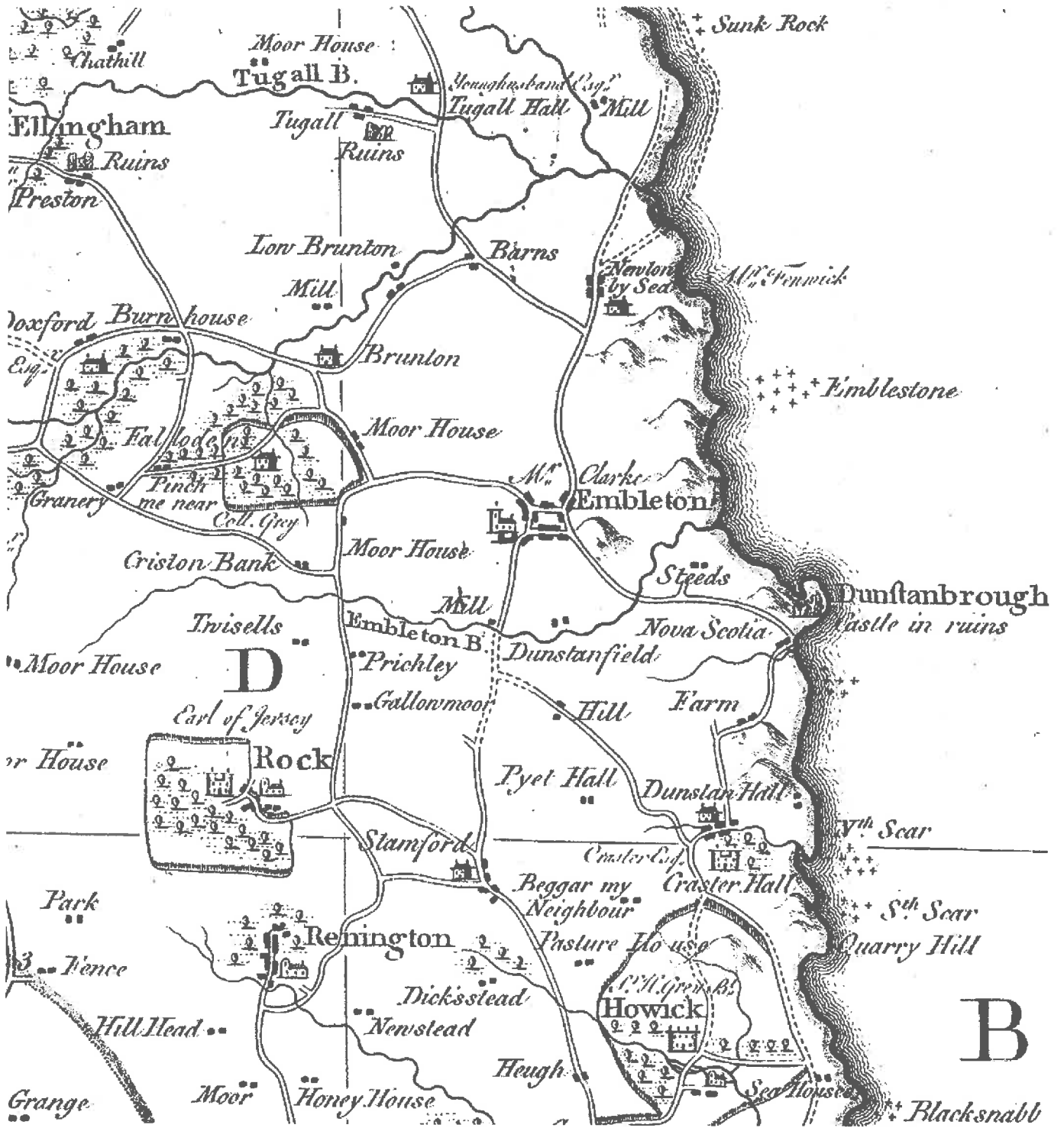
Legend

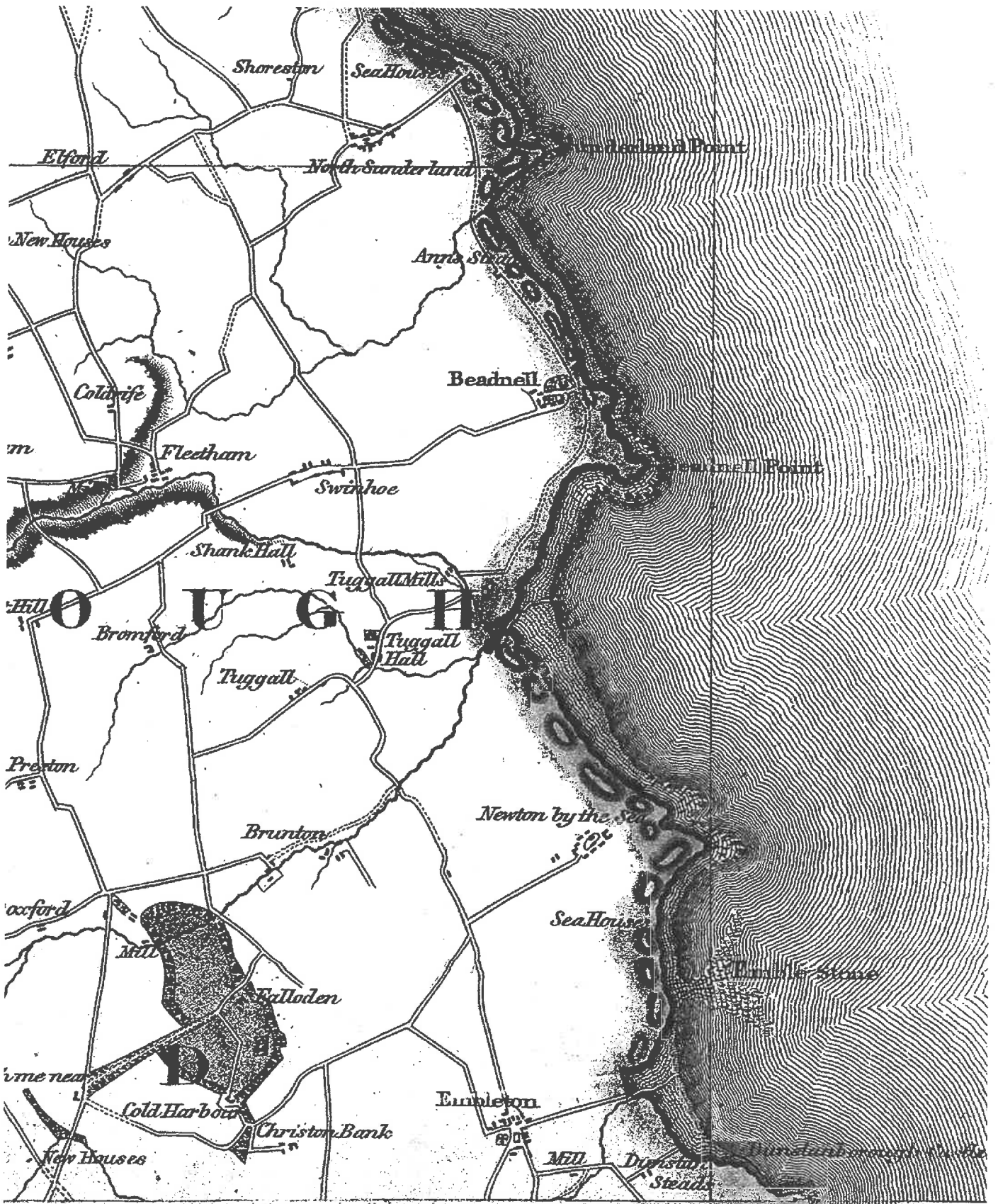
- Footpath
- Bridleway
- Restricted Byway
- Byway Open to All Traffic

This is a computer generated extract of the Working Copy of the Definitive Map of Public Rights of Way

Scale: 1:10,000

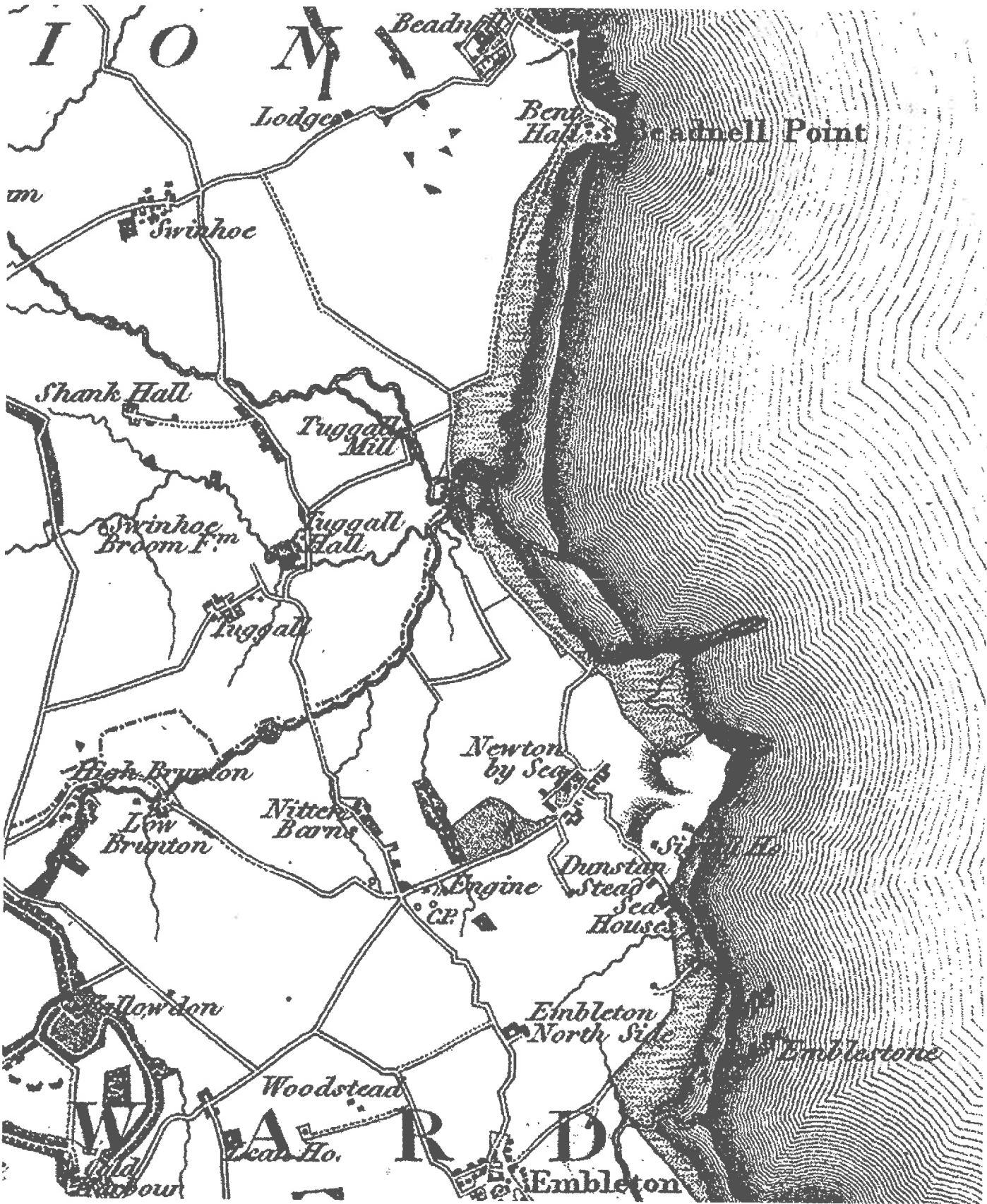
This map is reproduced from Ordnance Survey material with the permission of Ordnance Survey on behalf of the Controller of Her Majesty's Stationary Office Crown Copyright. Unauthorised reproduction infringes Crown Copyright and may lead to prosecution or civil proceedings. License no. 100049048 (2010).

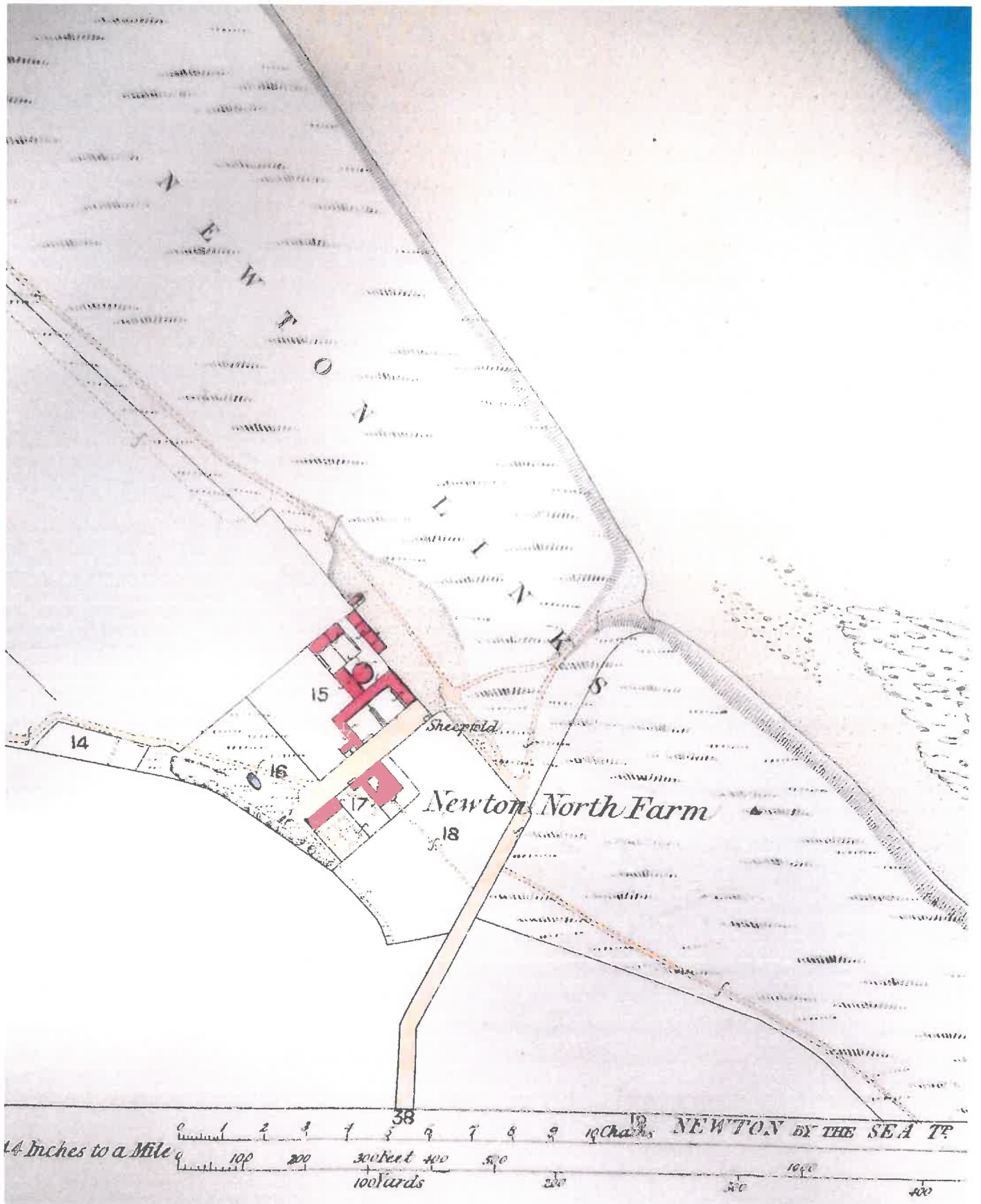




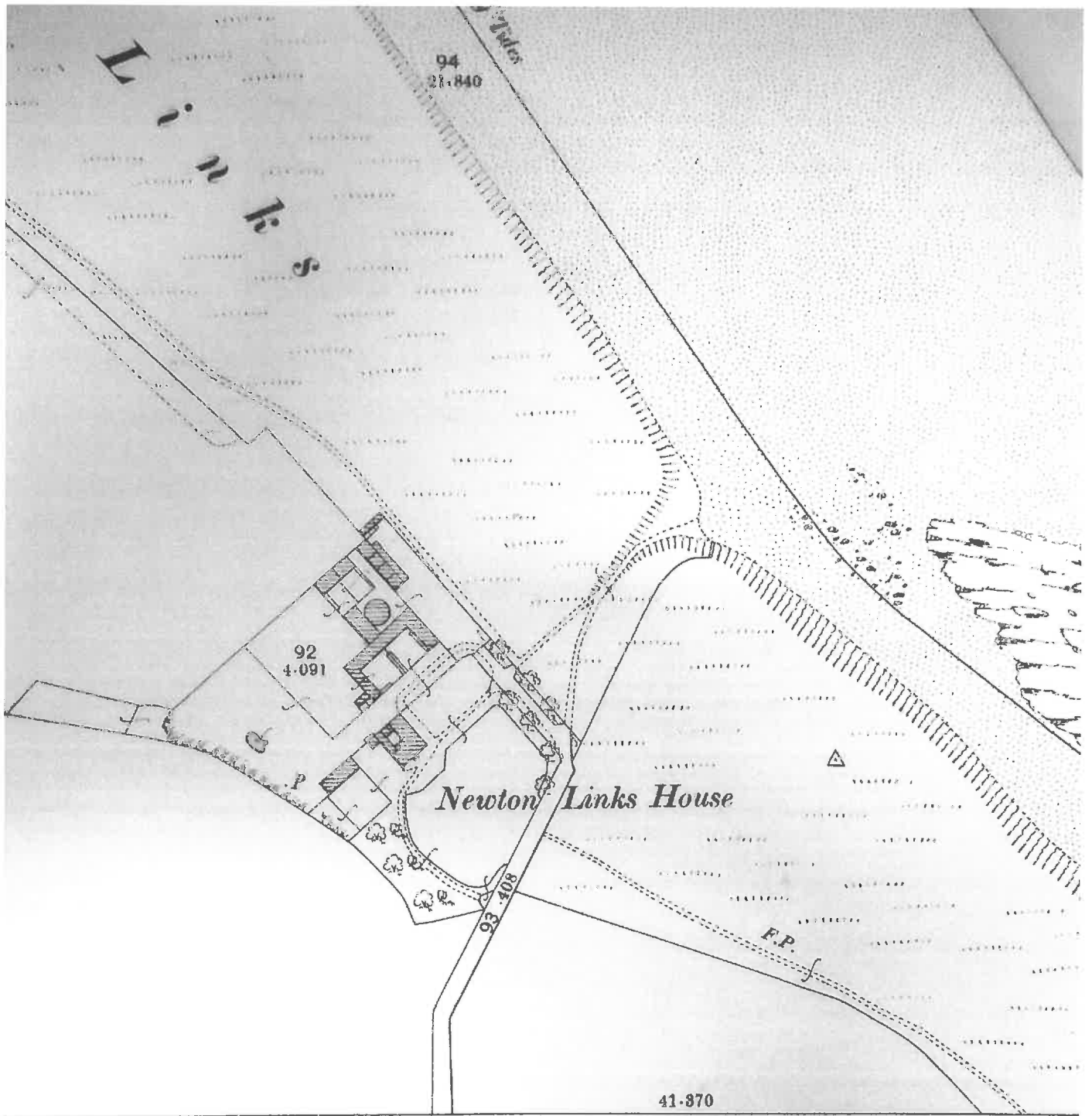


Greenwood's County Map
1828





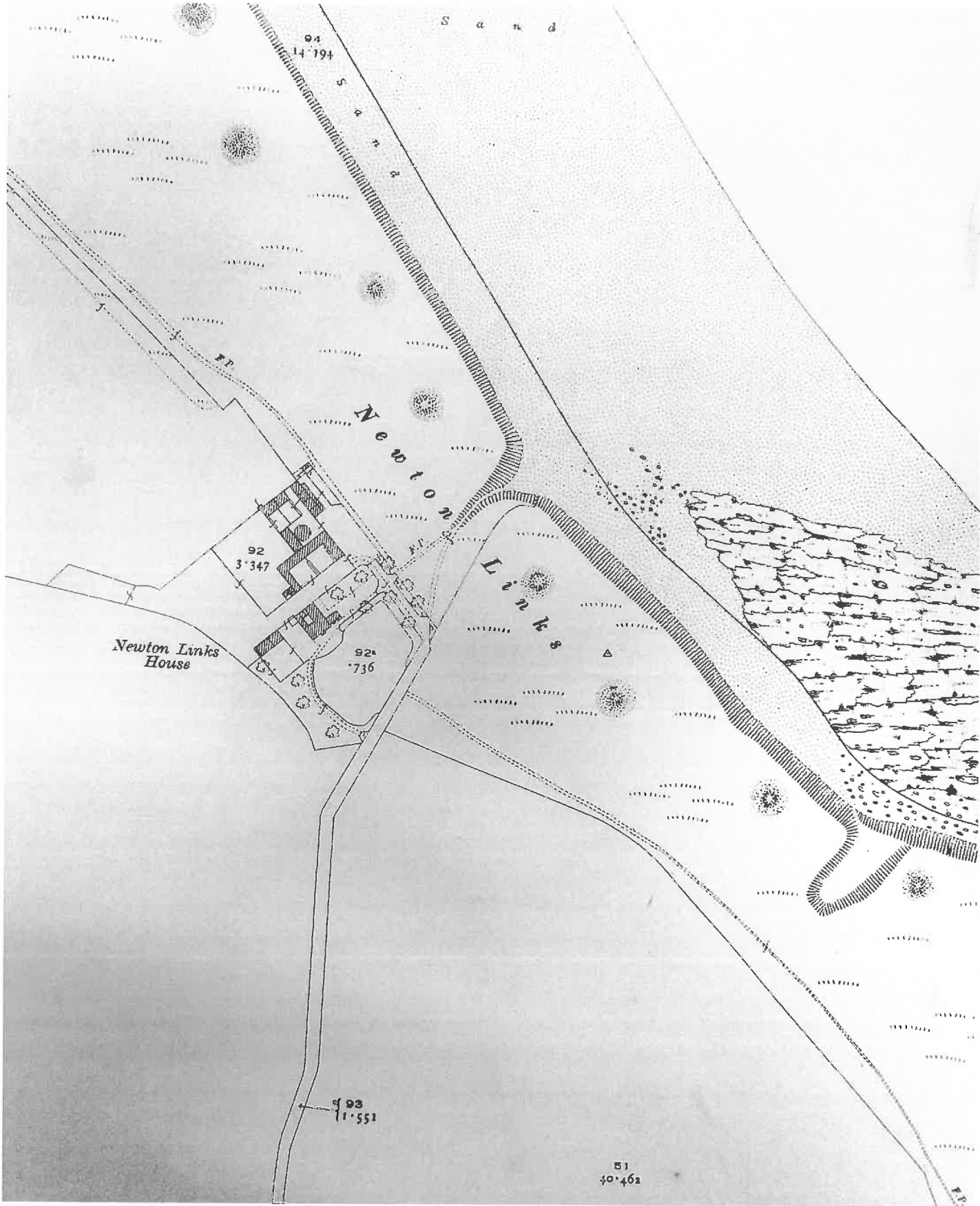
No. on Plan.	Area in Acres.	Description.	No. on Plan.	Area in Acres.	Description.
	717-049	<i>Brought forward.</i>		253-052	<i>Brought forward.</i>
70	-142	Wood.	9	14-694	Arable.
71	32-295	Arable, road, & stream	10	16-974	Arable & stream.
72	31-658	Arable, road, & stream	11	16-748	Arable.
73	-060	Pond.	12	46-435	Arable & stream.
74	-788	Wood & road.	13	15-248	Arable, stream, & road
75	27-288	Arable, road, & stream	14	-170	Garden.
76	1-966	Public road.	15	-899	Farmsteading.
77	29-013	Arable & stream.	16	-954	Rough pasture & road.
78	16-317	Arable & stream.	17	-346	Houses & gardens.
79	18-110	Arable.	18	1-368	Pasture.
80	-152	Pond.	19	42-486	Arable, rough pasture, quarry, stream, and small pond.
81	15-785	Arable.			
82	5-781	Wood.			
83	-053	Lodge & garden.	20	45-499	Rough pasture, sand, stream, & roads.
84	14-513	Arable.			
85	14-533	Arable.	21	58-596	Arable, rough pasture, & stream.
86	5-974	Arable.			
87	-568	Wood.	22	37-155	Arable, streams, and small pond.
88	18-856	Arable.			
89	20-717	Arable.	23	22-739	Arable.
			24	24-033	Arable.
	971-618		25	20-179	Arable.
			26	4-680	Arable.
			27	-031	Private road.
			28	-075	House & garden.
			29	1-433	Farmsteading & garden
			30	1-660	Rough pasture & road.
			31	-090	House, ruins, & gardens
			32	-352	Wood & pasture.
			33	-430	Garden.
			34	-463	House, offices, garden, & yards.
			35	-450	Wood & road.
			36	-285	Wood.
			37	3-329	Pasture.
			38	4-929	Public road.
			39	-075	House & garden.
			40	-721	Houses, gardens, and yards.
			41	2-480	Rough pasture & road.
			42	3-158	Arable.
			43	-049	Garden.
			44	-963	Houses & gardens.
			45	-244	Wood.
			46	-095	House & garden.
			47	2-485	Houses, gardens, and yards.
			48	-381	House & garden.
			49	10-927	Arable & road.
			50	42-204	Arable & road.
	253-052	<i>Carried forward.</i>		699-564	<i>Carried forward.</i>
RECAPITULATION.					
	947-998	Land.			
	1-330	Water.			
	12-170	Public roads.			
	10-120	Railway.			
	971-618	Total Area of the Tp. of Brunton.			
TP. OF NEWTON-BY-THE-SEA.					
1	2-030	Stream (half of).			
2	17-497	Arable & stream.			
3	13-584	Arable & stream.			
4	25-243	Arable, road, & stream.			
5	91-324	Rough pasture, sand, stream, & roads.			
6	8-559	Arable & rough pasture			
7	62-600	Arable & road.			
8	32-215	Arable & stream.			



51

CHARACTERISTICS AND SYMBOLS FOR BOUNDARIES, &c.

---	W	Change of Boundary, indicating the point at which the character of a Boundary changes		Every parcel is numbered thus Its area is given underneath in Acres, thus Braces indicating that the spaces so connected are included in the same reference number and area
---	D	Antiquities (Site of)		
---	P	Trigonometrical Station		

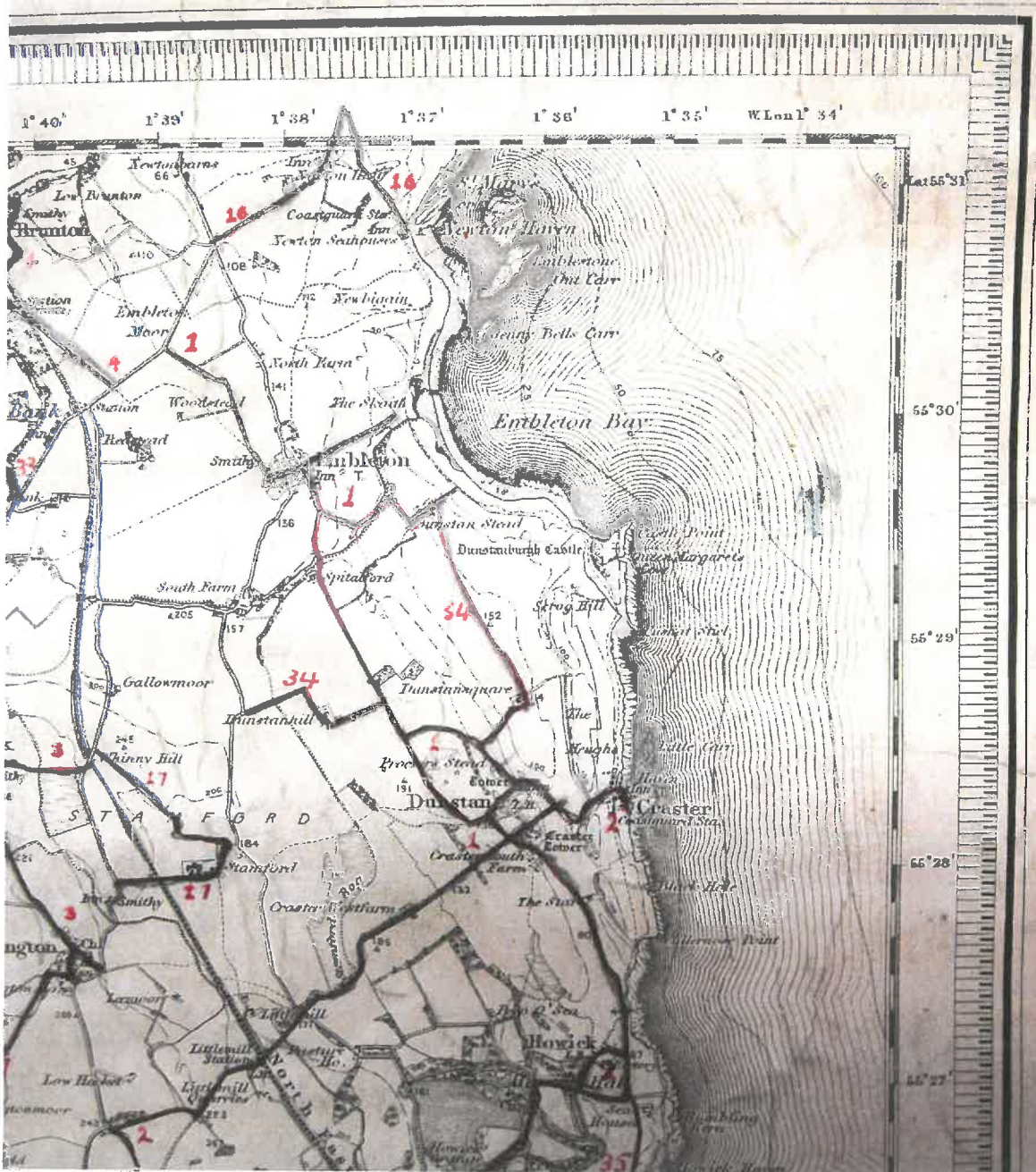


Alnwick RDC Handover Map
1932

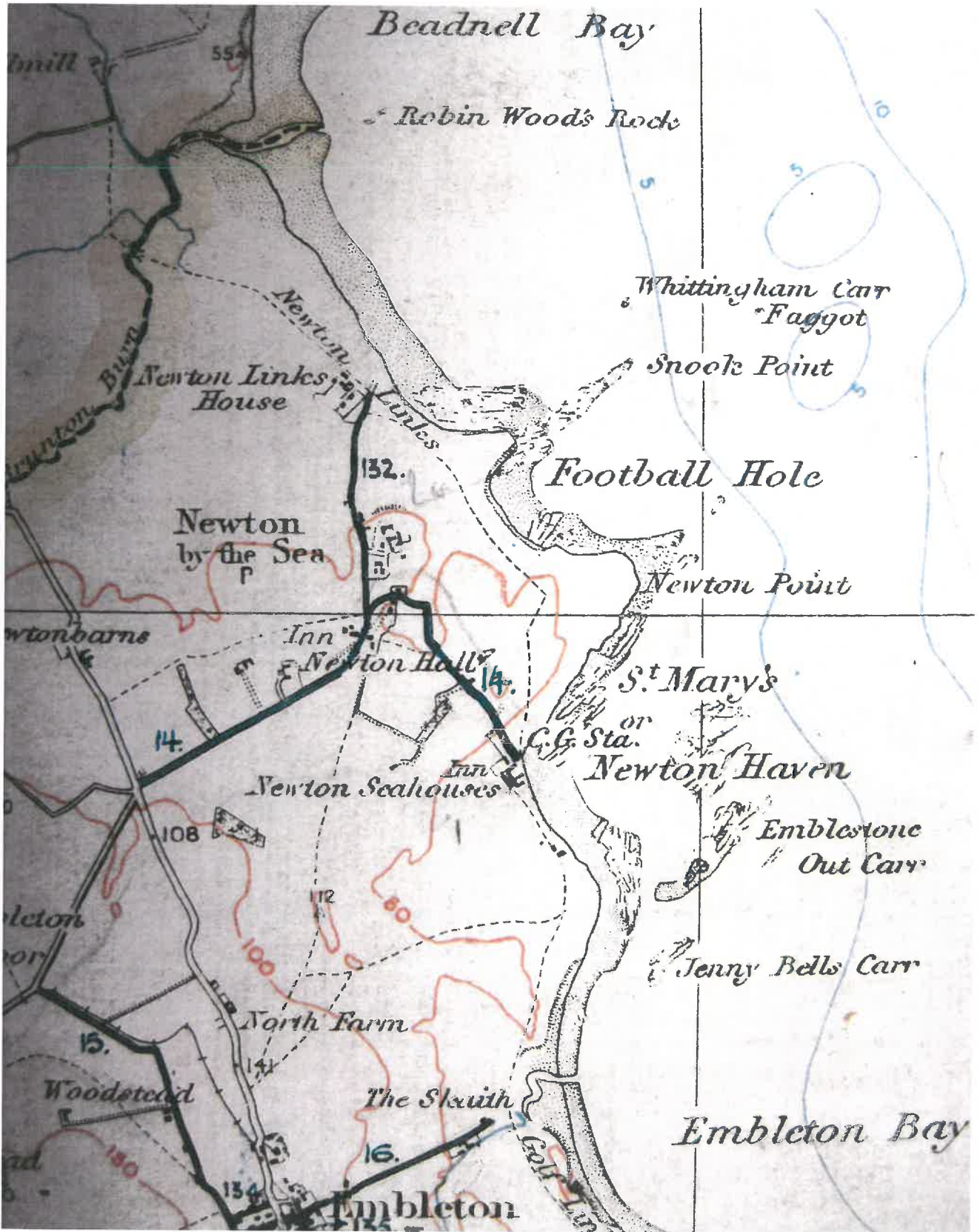
K. R. D. C.

EGATED ROADS. JUNE 1932

SHEET 6



Extract from Northumberland County Council One Inch Maps showing restrictions under Sections 1 and 2 of the Restriction of Ribbon Development Act 1935



Alnwick RD

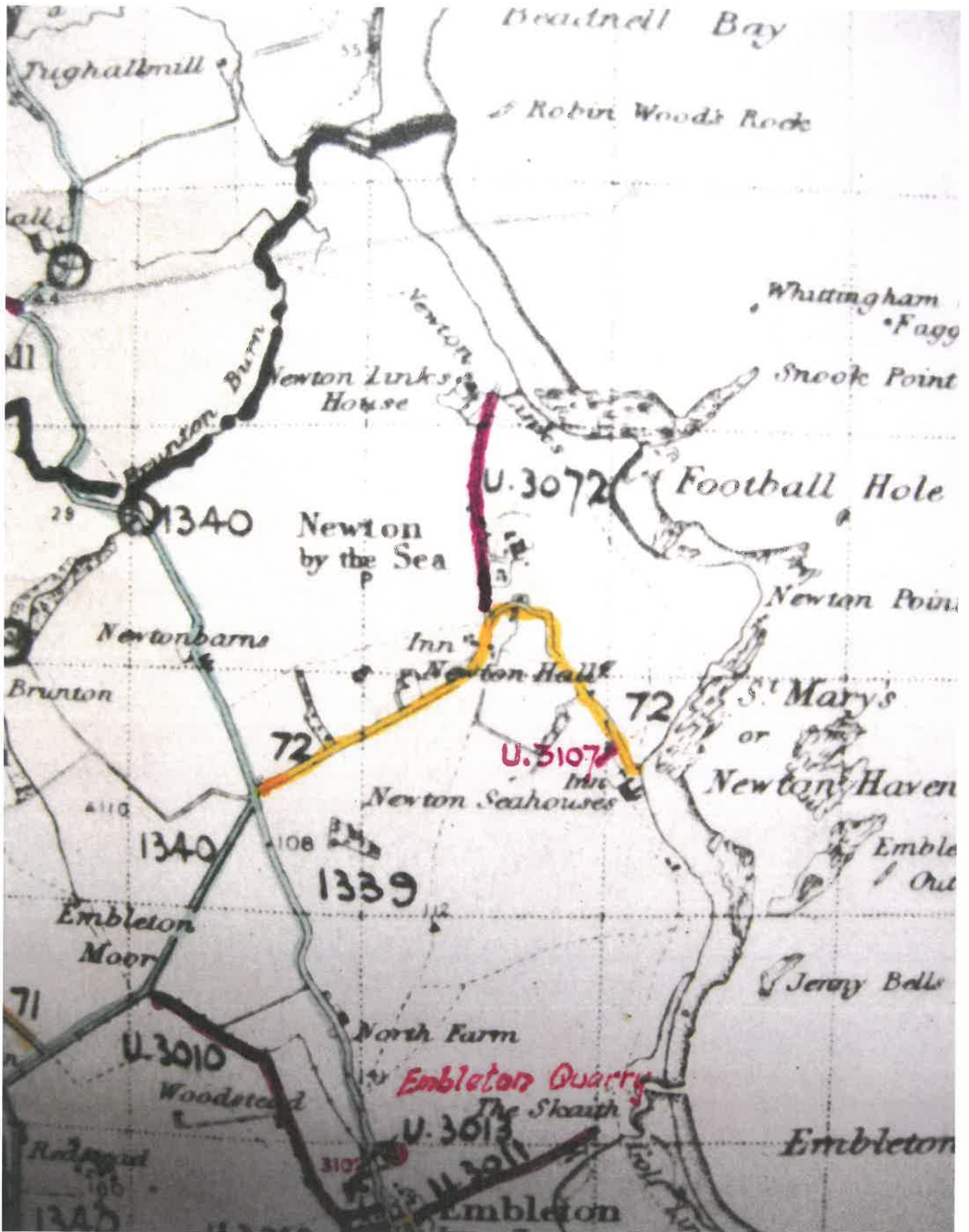
COUNTY OF NORTHUMBERLAND

Restriction of Ribbon Development Act 1935

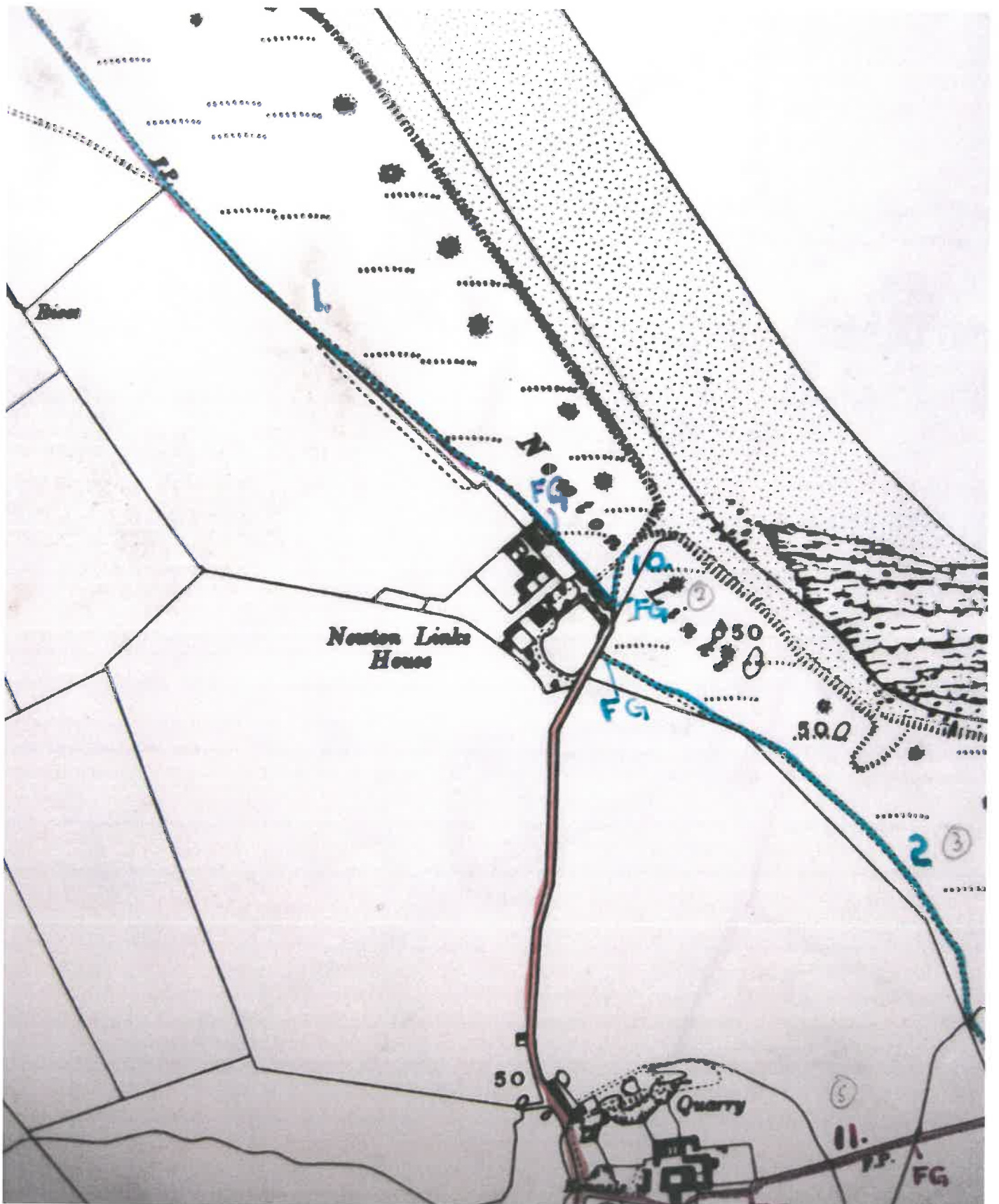
LINE OF UNCLASSIFIED ROADS IN THE RURAL DISTRICT OF ALN WICK IN RESPECT OF WHICH THE PROVISIONS OF SECTION 21 OF THE RESTRICTION OF RIBBON DEVELOPMENT ACT 1935 WERE ADAPTED ON 27th JUNE 1967 BY RESOLUTION OF THE BRIDGES AND ROADS COMMITTEE ACTING UNDER DELEGATED POWERS.

126. Road from the Shipley-Charlton Mires road B.6847 at South Charlton via Whimpy and Humberough to the Alnwick-Barwick road A.1 at Haleslaw Bank.
127. Road from the Alnwick-Barwick road A.1 at Heckley House via Humberough to a point on the Alnwick-Edinburgh road B.6946 at the north of Egar's Well Lodge.
128. Road from a point on the Alnwick-Barwick road A.1 200 yds. north of Humberough via Smiley Lane to a point on the Alnwick-Edinburgh road B.6946 1 mile north east of Abbyslands.
129. Road from the Alnwick-Barwick road A.1 at North Charlton via Humberough Moors to a point between 800 yds. north west of the site of Haleslaw.
130. Road from Lockydale Bridge via Carrintridge Bank, Botels Bridge, Krome Wood and Leamington Bank to the Hobbury-Alnwick road B.6861 at Banktop.
131. Road from the Hobbury-Embleton road B.1889 at Lesbury via Foxton Hall road and Mount Pleasant to its junction with the Alnwick-Edinburgh road B.1888 at Alnwick.

Extract from the Council's 1951 Highways Map



Survey Map
(Annotated c. 1951)



NATIONAL PARKS AND ACCESS TO THE COUNTRYSIDE ACT, 1949.
SCHEDULE ACCOMPANYING SURVEY MAPS.

Parish of Newton by Sea in the Rural District of

~~Borough~~
~~Urban District~~ } of Alnwick
(Delete whichever is inapplicable).

1. Number of highway on Map 1
2. Kind of Path (i.e., F.P., B.R.) F.P. + P.A.
3. Starts at Suspension Bridge to Newton Link House
4. Name of Path (if any) Link Road
5. Is the Path well defined? No
6. Is the Path metalled? If so, define length No
7. If its width can be stated, insert here _____
8. What is the present condition of the path, stiles, etc.? Grass Track
9. Is it subject to being ploughed out? No
10. Details of any notice boards, direction signs or warning signs against trespassers, stating their location, wording on them, their condition and date of erection, if known
Nil
11. Grounds for believing the path to be public (if known), e.g. "Awarded," "Repaired at Public Expense" (with date) or "mentioned in Minutes of Parish Council" or any other evidence such as that of an old inhabitant

Awarded Old established path

12. Have persons been prevented using the highway? No
13. Give particulars of any obstructions Nil
14. Names of owners of freehold and previous owners, if known, for past 30 years
G. Bolam
Newton Link House
15. What maps have been consulted, and where are they deposited?
Nil
16. What records have been consulted, and where are they deposited?

17. Any other relevant information Well defined track from Sun Bridge to Primrose Hill, Newton Point field gate, then S.W. to Newton Sea House Road.

Surveyed by	Address	Dates of Survey
<u>D. B. Gregory</u>	<u>Newton by Sea</u>	<u>June 1/51</u>
<u>J. E. Blair</u>	<u>Newton by Sea</u>	<u>June 1/51</u>
<u>Wm Middlemas</u>	<u>Newton by Sea</u>	<u>June 1/61</u>

Important:—Sheet No. of Map on which Highway is shown

19 S 1 47 18 SE 1
19 S 1 47 23 NE 2

NATIONAL PARKS AND ACCESS TO THE COUNTRYSIDE ACT, 1949.
SCHEDULE ACCOMPANYING SURVEY MAPS.

Parish of Newton by Sea in the Rural District of _____

Borough }
Urban District } of Alnwick
(Delete whichever is inapplicable).

1. Number of highway on Map 2 2. Kind of Path (i.e., F.P., B.R.) FP+BR
3. Starts at Newton Link House to Newton Sea House
4. Name of Path (if any) _____
5. Is the Path well defined? no
6. Is the Path metalled? If so, define length no
7. If its width can be stated, insert here _____
8. What is the present condition of the path, stiles, etc.? Grass Track with Field Gates
9. Is it subject to being ploughed out? no
10. Details of any notice boards, direction signs or warning signs against trespassers, stating their location, wording on them, their condition and date of erection, if known _____

11. Grounds for believing the path to be public (if known), e.g. "Awarded," "Repaired at Public Expense" (with date) or "mentioned in Minutes of Parish Council" or any other evidence such as that of an old inhabitant

Awarded Old Established Path

12. Have persons been prevented using the highway? no
13. Give particulars of any obstructions nil
14. Names of owners of freehold and previous owners, if known, for past 30 years Sir Arthur M Sutherland

15. What maps have been consulted, and where are they deposited? _____

16. What records have been consulted, and where are they deposited? nil

17. Any other relevant information Continuation from 7241.

Surveyed by	Address	Dates of Survey
<u>D E Gregory</u>	<u>Newton by Sea</u>	<u>June 1/51</u>
<u>J E Blair</u>	<u>Newton by Sea</u>	<u>June 1/51</u>
<u>Wm Middleton</u>	<u>Newton by Sea</u>	<u>June 1/51</u>

Important:—Sheet No. of Map on which Highway is shown

1951 47 23 NE 1

**NATIONAL PARKS AND ACCESS TO THE COUNTRYSIDE ACT, 1949.
SCHEDULE ACCOMPANYING SURVEY MAPS.**

Parish of Newton by Sea in the Rural District of _____

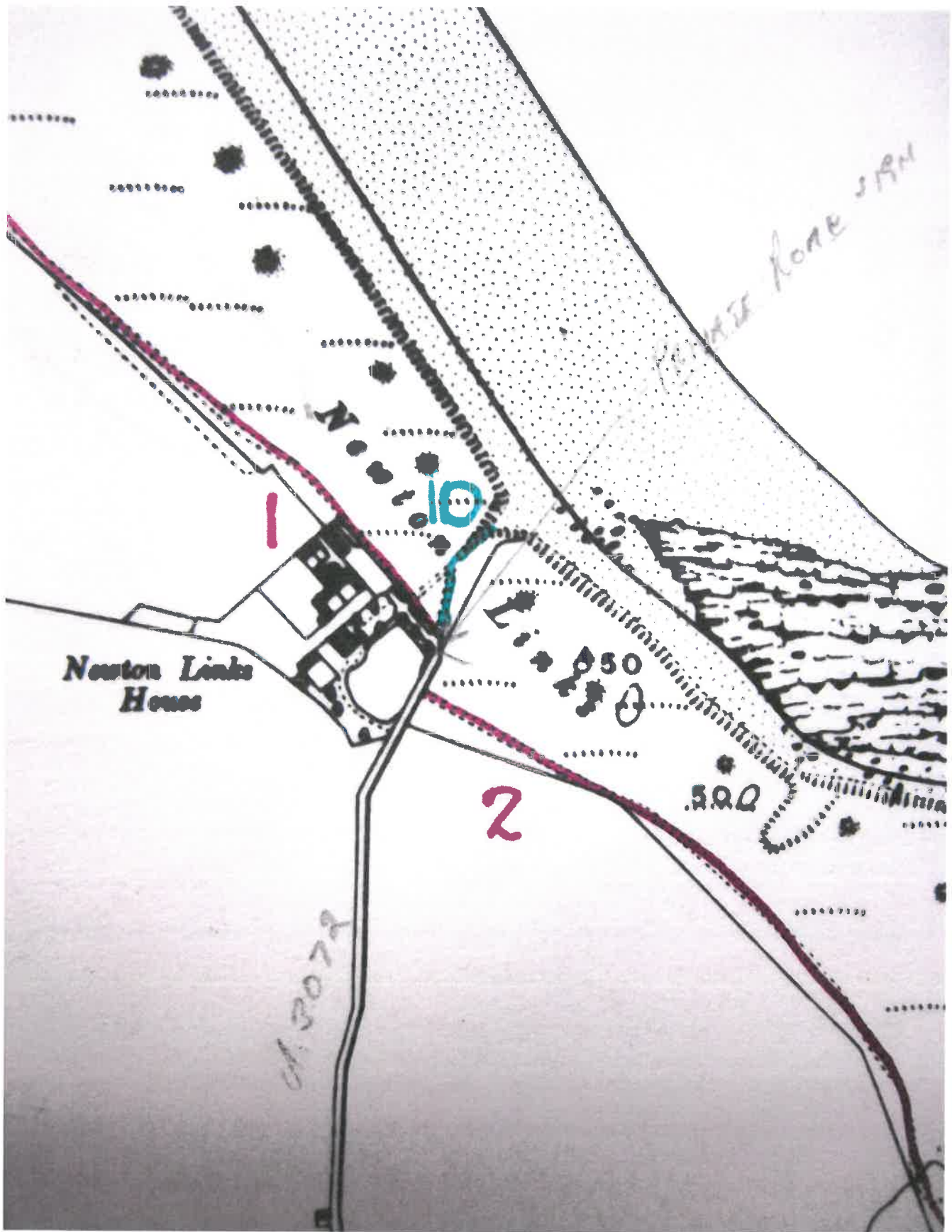
~~Borough~~
~~Urban District.~~ } of Alnwick
(Delete whichever is inapplicable).

1. Number of highway on Map 10 2. Kind of Path (i.e., F.P., B.R.) C.R.B.
3. Starts at Newton Link House to Sea
4. Name of Path (if any) _____
5. Is the Path well defined? yes
6. Is the Path metalled? If so, define length no
7. If its width can be stated, insert here. _____
8. What is the present condition of the path, stiles, etc.? Sand Track
9. Is it subject to being ploughed out? no
10. Details of any notice boards, direction signs or warning signs against trespassers, stating their location, wording on them, their condition and date of erection, if known
nil
11. Grounds for believing the path to be public (if known), e.g. "Awarded," "Repaired at Public Expense" (with date) or "mentioned in Minutes of Parish Council" or any other evidence such as that of an old inhabitant
Awarded Old Track
12. Have persons been prevented using the highway? no
13. Give particulars of any obstructions nil
14. Names of owners of freehold and previous owners, if known, for past 30 years
Mr G. Bolam
15. What maps have been consulted, and where are they deposited? _____
16. What records have been consulted, and where are they deposited? _____
17. Any other relevant information Old road to Sea Shore
Sand Track.

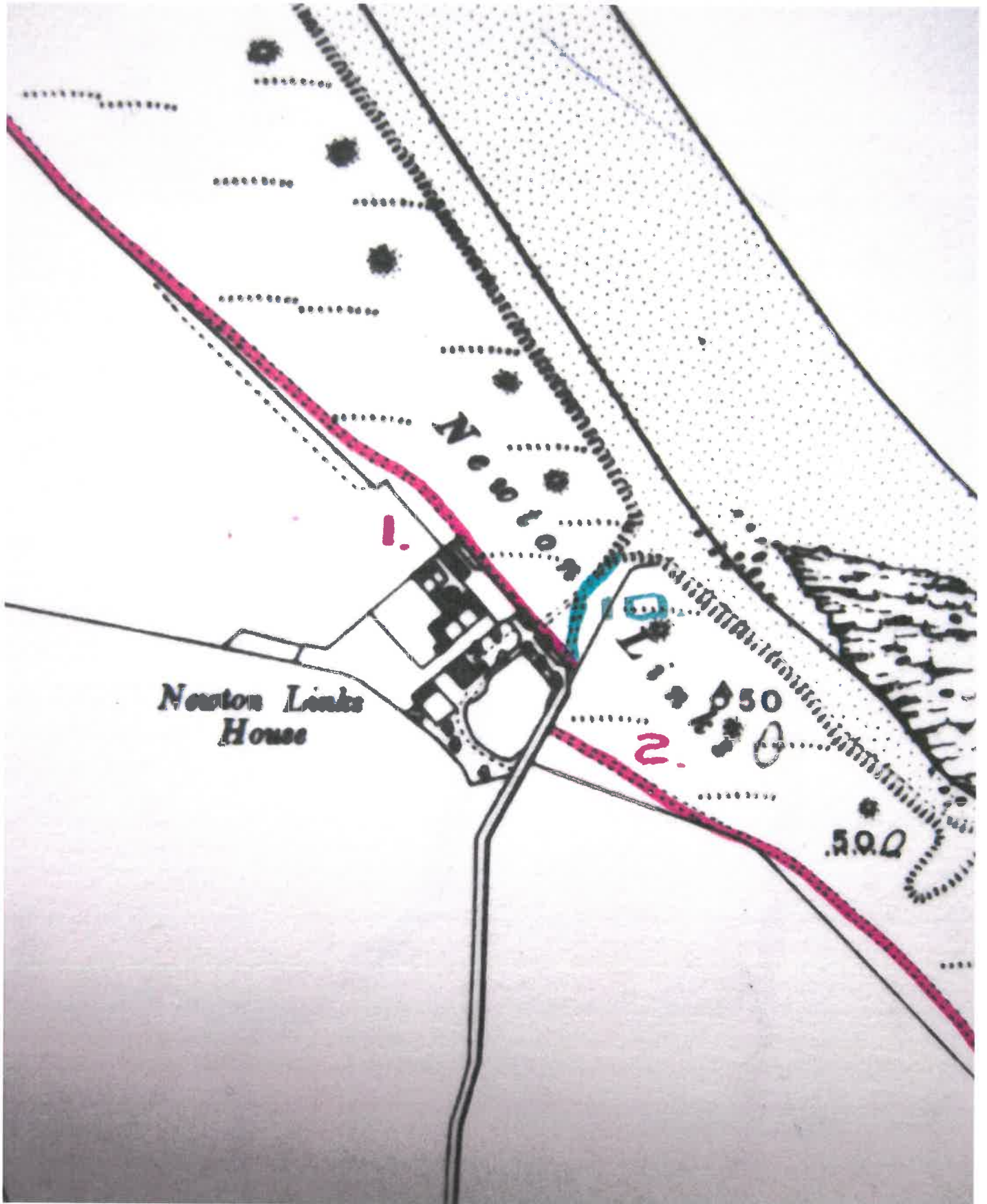
Surveyed by	Address	Dates of Survey
<u>D E Gregory</u>	<u>Newton by Sea</u>	<u>June 4/51</u>
<u>G E Blair</u>	<u>Newton by Sea</u>	<u>June 4/51</u>
<u>Edm Middlemiss</u>	<u>Newton by Sea</u>	<u>June 4/51</u>

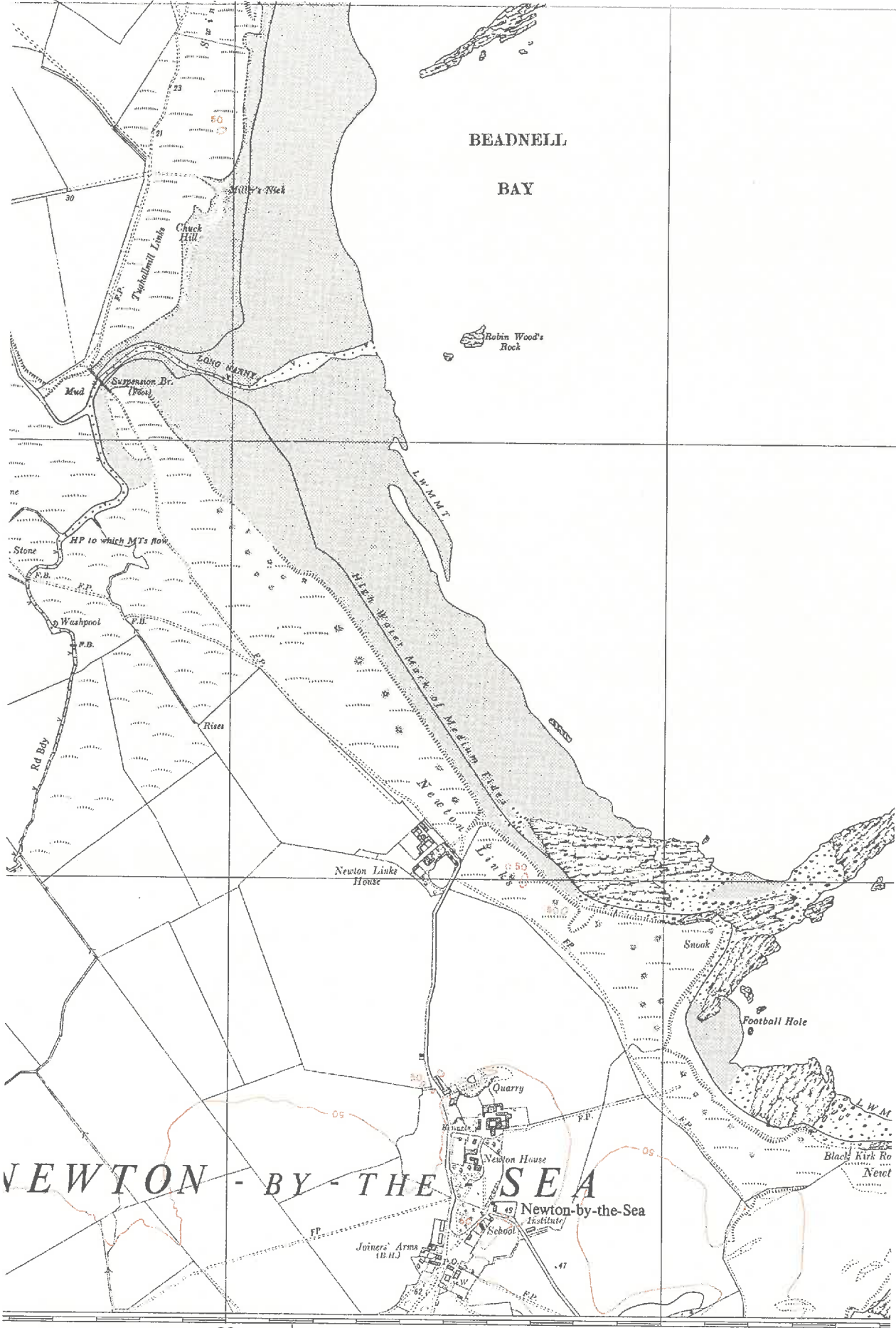
Important:—Sheet No. of Map on which Highway is shown

195 1 47 23 NE2



Provisional Map





1958 County Road Schedule

BELFORD DIVISION

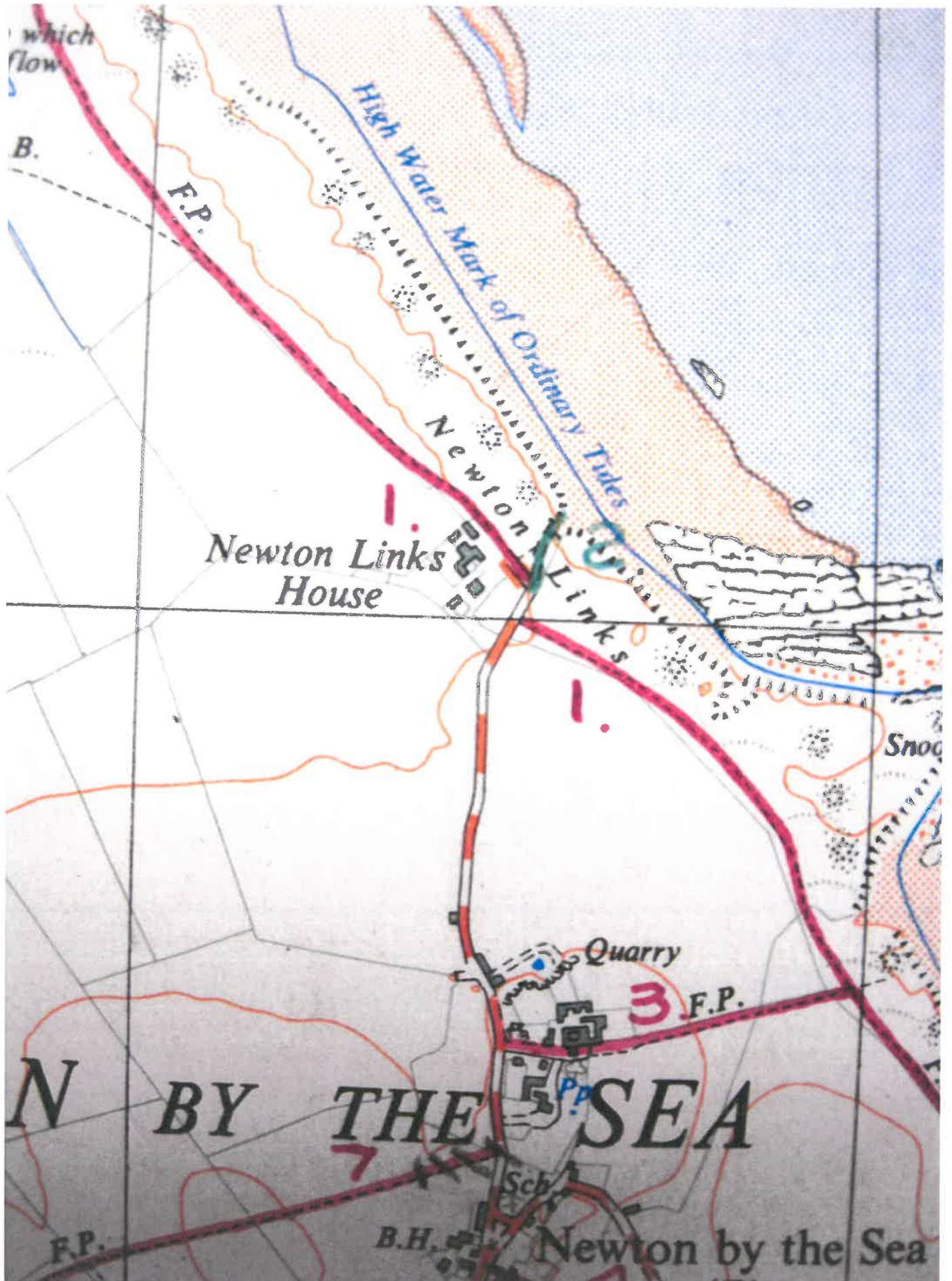
Unclassified Roads in Alnwick Rural District

Brought Forward IL.855

U.3062	Crawley Tower Road	From A.697 at Powburn via Crawley Tower to C.83 near Hedgeley Hall.	0.90
U.3063	Low Hedgeley Road	From A.697 to Low Hedgeley.	0.21
U.3064	Shepherd's Law - Beenley Harehope Road	From C.84 near Shepherd's Law crossing C.82 at Beanley to Rural District Boundary at Waterside Plantation near Harehope.	2.11
U.3065	Eglington Moor Road	From B.6346 at Eglington via Tarry to Rural District Boundary Eglington Moor.	0.86
U.3066	Harehope Road	From B.6346 to Harehope.	0.28
U.3067	Titlington Farm Road	From C.83 to Titlington Farm.	0.36
U.3068	Eglington - Ditchburn Smallburns Road.	From B.6346 near Eglington Church via West Ditchburn to B.6346 at Smallburn.	3.16
U.3070	East Ditchburn Road	From B.6347 at South Charlton towards East Ditchburn, farm gate.	2.174
U.3072	Links Road Newton	From C.72 at Newton by the Sea to Newton Links House.	0.57
U.3073	Road to Post Office See View.	From C.74 Craster at Post Office, northwards.	0.11
U.3089	Burnhouses Rock	From C.73 at a point 70 yards north of U.3009 westwards for 152 yards.	0.087
U.3090	Main Street, Craster	From C.74 at junction with U.3073, southwards into Eugh Estate to House No. 34, and including link to west serving houses No. 7-16.	0.41
U.3091	Butchers Lane, Craster.	From U.3090 at Jolly Fisherman, 250 feet westwards.	0.047
U.3094	Shipley Lane.	From B.6346, 260 yards east of junction with C.85 northwards for 150 yards.	0.085

23.218

Carried forward



NORTHUMBERLAND COUNTY COUNCIL.

NATIONAL PARKS AND ACCESS TO THE COUNTRYSIDE ACT, 1949.
PART IV.

PUBLIC RIGHTS OF WAY - STATEMENT.

1. Borough
Urban District
Rural District AINWICK.....
2. Parish NEWTON-BY-THE-SEA.....
3. Number of Footpath on Map 1.....
4. Name of Path
5. Kind of Path (i.e. FP/BR) F.P.....
6. General Description of Path From F.P. 5 at the Beadnell Parish boundary
at the Suspension Bridge over Long Nanny in a South-easterly and southerly
direction by the public road at Newton Links House and crossing the road at
Newton Seahouses to join F.P.1 at the Embleton Parish boundary.
.....
.....
7. Other relevant information
-
-
-
-
-

NORTHUMBERLAND COUNTY COUNCIL

NATIONAL PARKS AND ACCESS TO THE COUNTRYSIDE ACT, 1949

PART IV

PUBLIC RIGHTS OF WAY - STATEMENT

1. Borough
Urban District
Rural District
ALNWICK
2. Parish
NEWTON-BY-THE-SEA
3. Number of Footpath on Map 2
4. Name of path
5. Kind of Path (i.e. FP/BR) B.R.
6. General Description of Path
From the public road at Newton Links House
in a northerly direction across Newton Links to the Sea.
.....
.....
.....
.....
7. Other relevant information
.....
.....
.....
.....
.....

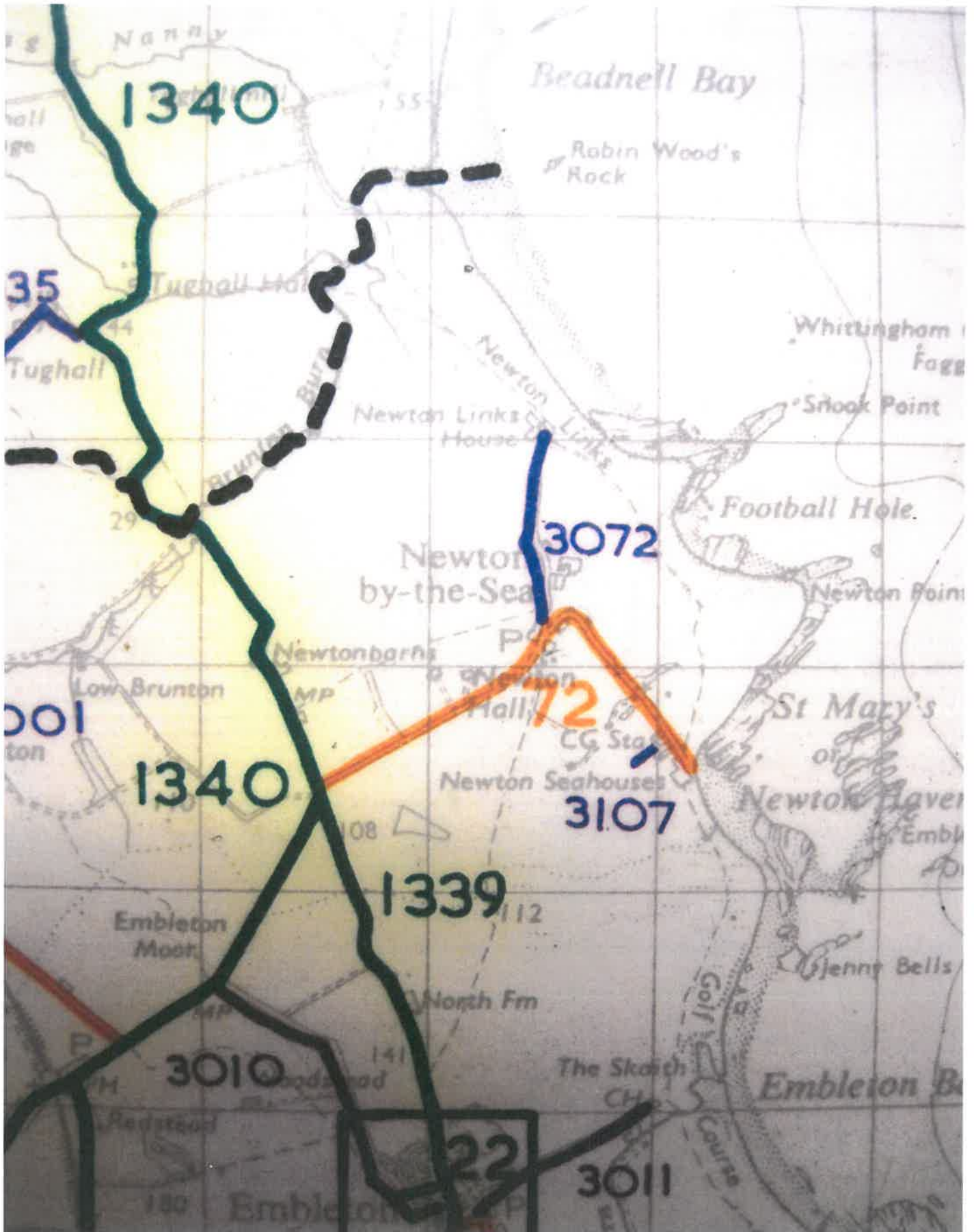


1964 County Road Schedule

- 82 -

<u>Route No.</u>	<u>Name of Road.</u>	<u>Description.</u>	<u>Responsible Division or Authority.</u>	<u>Mileage.</u>	<u>Total Mileage.</u>
U.3060	Shawdon Hill Road.	From A.697 near Shawdon Hill north-eastwards to C.85 near Shawdon Hall.	Alnwick.	0.56	0.56
U.3061	Shawdon Hill-Rothill Road.	From A.697 near Shawdon Hill south-westwards to Rural District boundary. (Continues in Rothbury Rural District as U.4001).	Alnwick.	0.15	0.15
U.3062	Crawley Tower Road.	From A.697 at Powburn eastwards via Crawley Tower to C.83 near Hedgeley Hall.	Alnwick.	0.90	0.90
U.3063	Low Hedgeley Road.	From A.697 near Brandon White House eastwards to Low Hedgeley.	Alnwick.	0.21	0.21
U.3064	Shepherd's Law-Beanley-Harehope Road.	From C.84 near Shepherd's Law northwards, crossing C.82 at Beanley to the Rural District boundary at Waterside Plantation near Harehope Hall. (Continues in Glendale Rural District as U.1074).	Alnwick.	2.11	2.11
U.3065	Eglington Moor Road.	From B.6346 at Eglington north-eastwards via Tarry to the Rural District boundary at Eglington Moor.	Alnwick.	0.86	0.86
U.3066	Harehope Road.	From B.6346 west of Eglington, north-eastwards to Harehope.	Alnwick.	0.20	0.20
U.3067	Titlington Farm Road.	From C.83 to Titlington Farm.	Alnwick.	0.17	0.17
U.3068	Eglington-Ditchburn-Smallburns Road.	From B.6346 near Eglington Terrace via West Ditchburn to B.6346 at Smallburns.	Alnwick.	3.16	3.16
U.3069	White House Folly Road.	From B.6347 south of Charlton House southwards via White House Folly to join B.6346 near White House.	Alnwick.	1.50	1.50
U.3070	East Ditchburn Road.	From B.6347 at South Charlton north-westwards via Brockleyhall Moor to East Ditchburn Farm Gate.	Alnwick.	2.17	2.17
U.3071	South Charlton-Heiferlaw Bank Road.	From B.6347 at South Charlton, southwards via Humbleheugh to join A.1 at Heiferlaw Bank.	Alnwick.	1.76	1.76
U.3072	Newton Links Road.	From C.72 at Newton-by-the-Sea northwards to Newton Links House.	Alnwick.	0.57	0.57

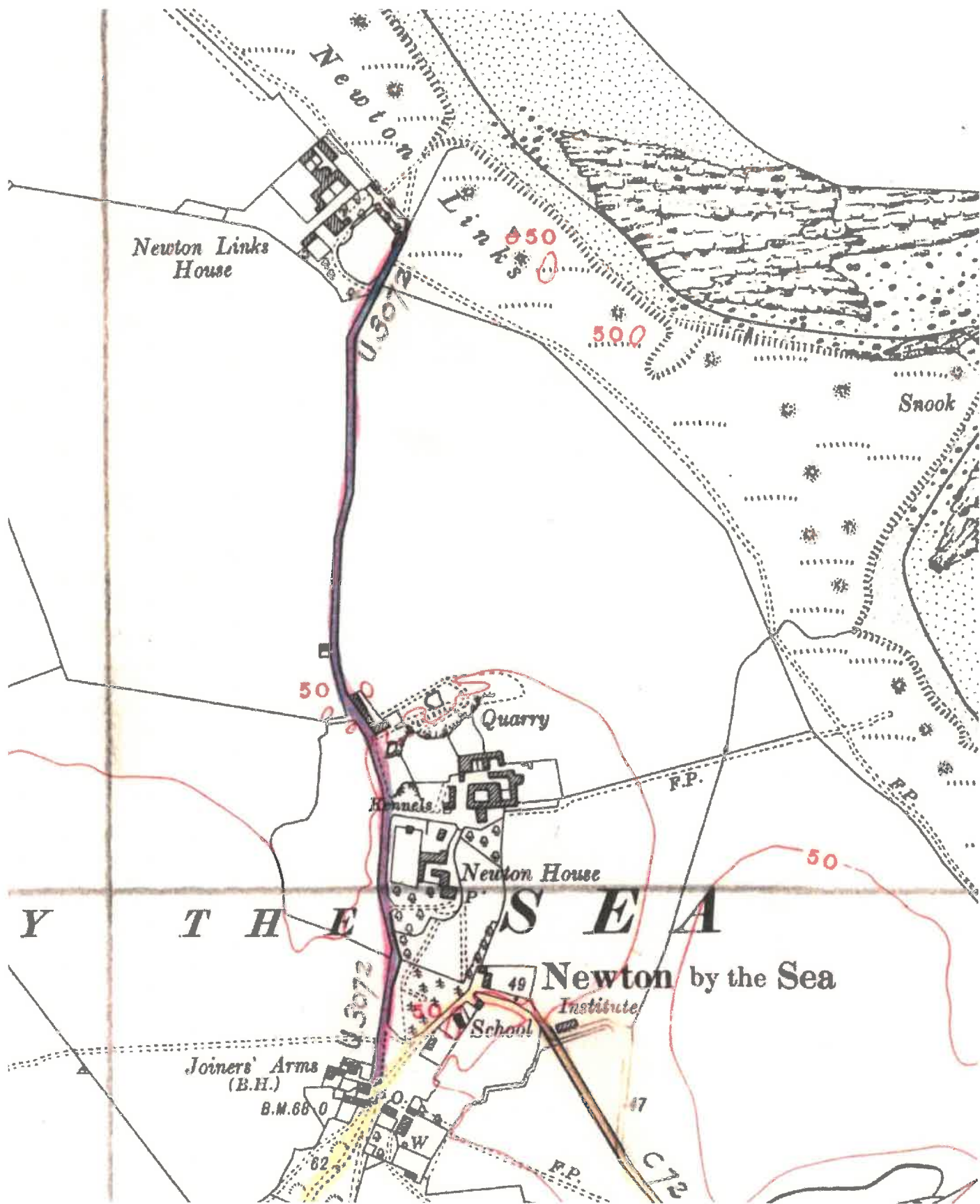
Extract from the Council's 1964 Highways Map



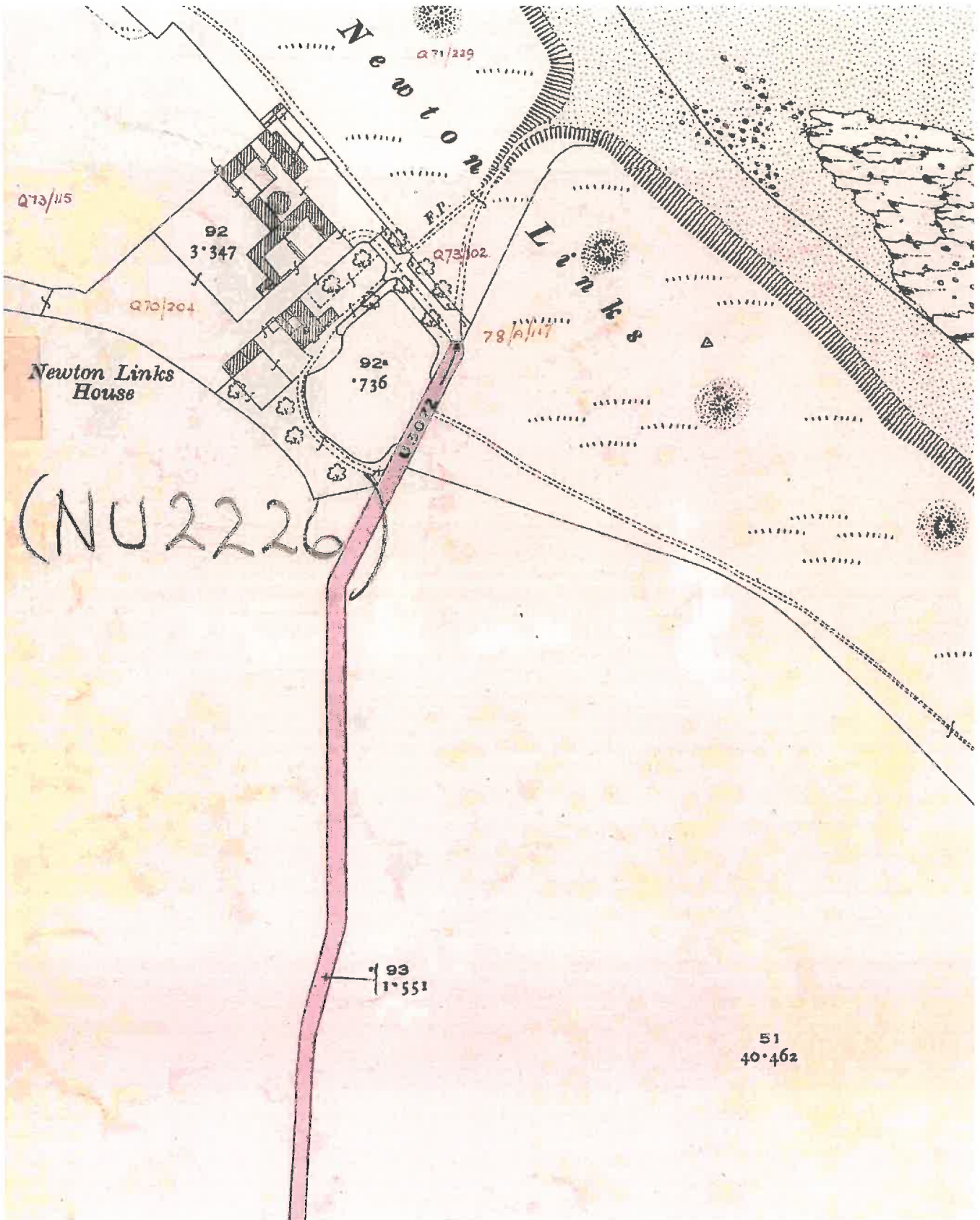
1974 County Road Schedule

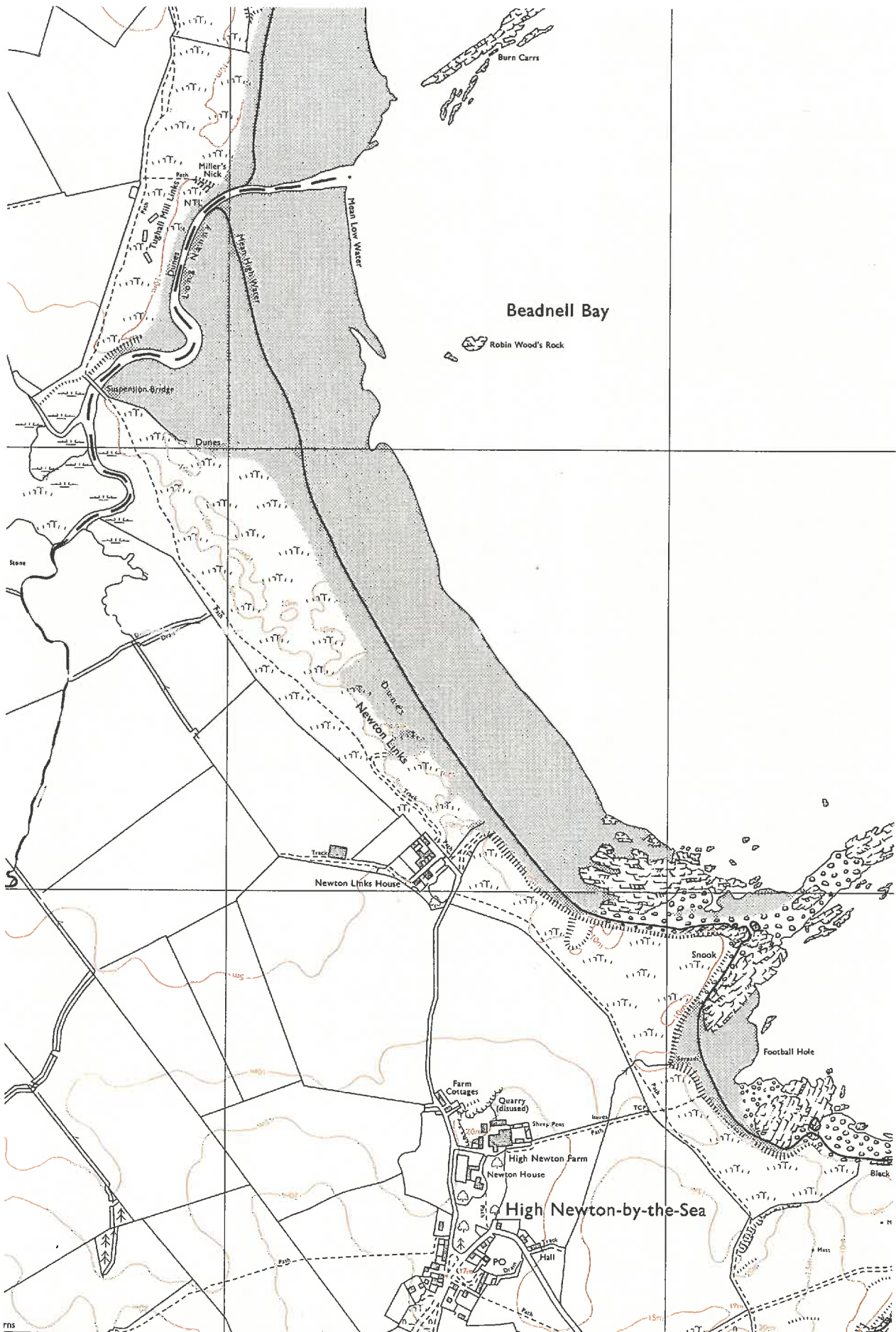
Route No.	Name of Road	Description	Responsible Division or Authority	Mileage	Total Mileage
U.3069	White House Folly Road.	From B.6347 south of Charlton House (NU 161192) southwards via White House Folly to join B.6346 near White House. (NU 159171).	Alnwick Division.		1.50
U.3070	East Ditchburn Road.	From B.6347 at South Charlton (NU 163202) northwards via Brockleyhall Moor to East Ditchburn Farm Gate. (NU 140215).	Alnwick Division.		2.17
U.3071	South Charlton - Heiferlaw Bank Road.	From B.6347 at South Charlton (NU 165202) southwards via Humbleheugh to join A.1 at Heiferlaw Bank. (NU 181183).	Alnwick Division.		1.76
U.3072	Newton Links Road.	From C.72 at Newton-by-the-Sea (NU 235252) northwards to Newton Links House. (NU 235260).	Alnwick Division.		0.57
U.3073	Craster Road.	From C.74 at Craster Post Office (NU 258199) northwards for 200 yards.	Alnwick Division.		0.11
U.3074	Hipsburn Crescent, Lesbury.	From U.3022 at Hipsburn (NU 233112) south-eastwards for 140 yards.	Alnwick Division.		0.08
U.3075	Streets at Felton.	Off A.1 at Felton Village the following streets (NU 185009) Davison Avenue, cul-de-sac. The Oval (138 yds.); South Lane (56 yds.); South View (133 yds.).	Alnwick Division.		0.34
U.3076	Falloden Avenue, Shilbottle.	From Kiln Lonnen, Shilbottle (U.3079) westwards to join C.95 near 'Hillcrest' including cul-de-sac off south side (200 yds.) (NU 202085).	Alnwick Division.		0.11
U.3077	Acklington-Broomhill Old Road.	From B.6345 at Acklington eastwards to C.104 east of Acklington (NU 234019).	Alnwick Division.		0.11

Extract from the Council's 6" Highways Map



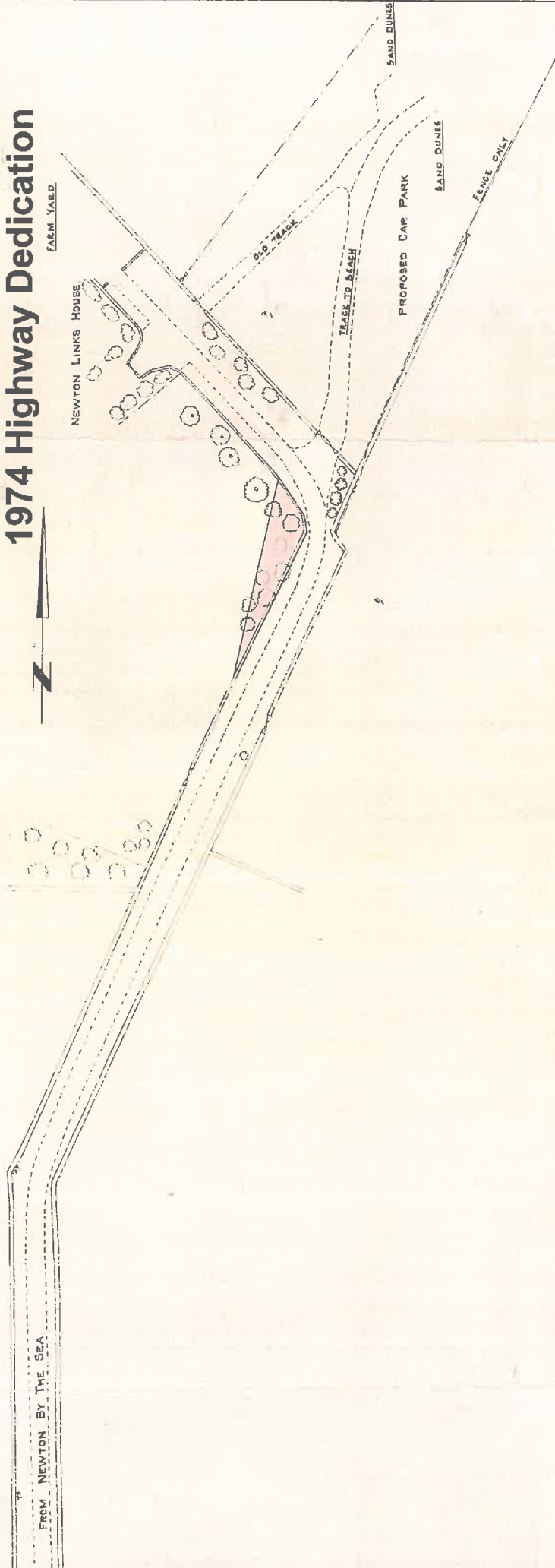
Extract from the Council's 25" Highways Map





1974 Highway Dedication

FARM YARD



1	SE-ALIGNMENT AT JUNCTION	28 1 73
No.	AMENDMENT	DATE
COUNTY OF NORTHUMBERLAND		
NEWTON LINKS ROAD U3072 PROPOSED IMPROVEMENT AT NEWTON LINKS HOUSE		
DRAWN	SCALES	DRAWING No.
TRACED	1/500	U3/263
CHECKED	DATE	
S. ARTHUR, M.Sc., B.Sc., C.Eng., M.I.C.E., F.I.Mun.E., M.Inst.H.E., COUNTY SURVEYOR PHOENIX HOUSE, QUEEN STREET, NEWCASTLE UPON TYNE NE1 3AT		

I/WE *Charles Thompson (care solicitor 200 Avenue) SA
Garry Crow, Catherine Banks, Arthur
McEntomash SA*

DO HEREBY DECLARE THAT I/WE HAVE DEDICATED TO THE USE OF THE PUBLIC AS A HIGHWAY THAT PORTION OF LAND SITUATED OFF U3072 ROAD AT 'NEWTON LINKS HOUSE, NEWTON BY THE SEA IN THE COUNTY OF NORTHUMBERLAND WHICH IS COLOURED RED HEREON AS FROM TODAY'S DATE

DATED THIS *16th* DAY OF *FEB* 197*4*
 SIGNED *K. Thompson,
Counsellor,
High Newton by the Sea,
Northumb.*

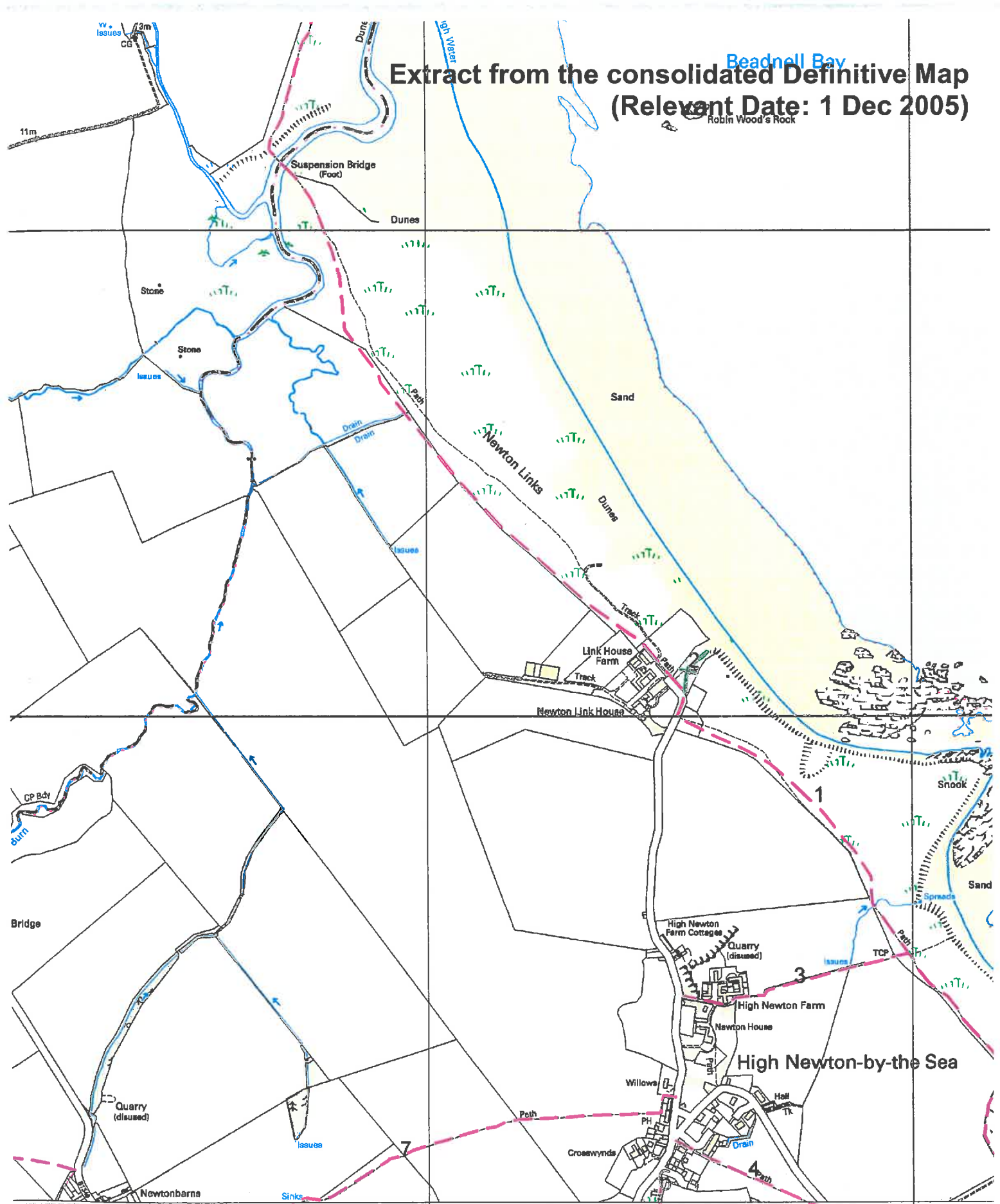
WITNESSES *[Signature]
Arthur, McEntomash SA*

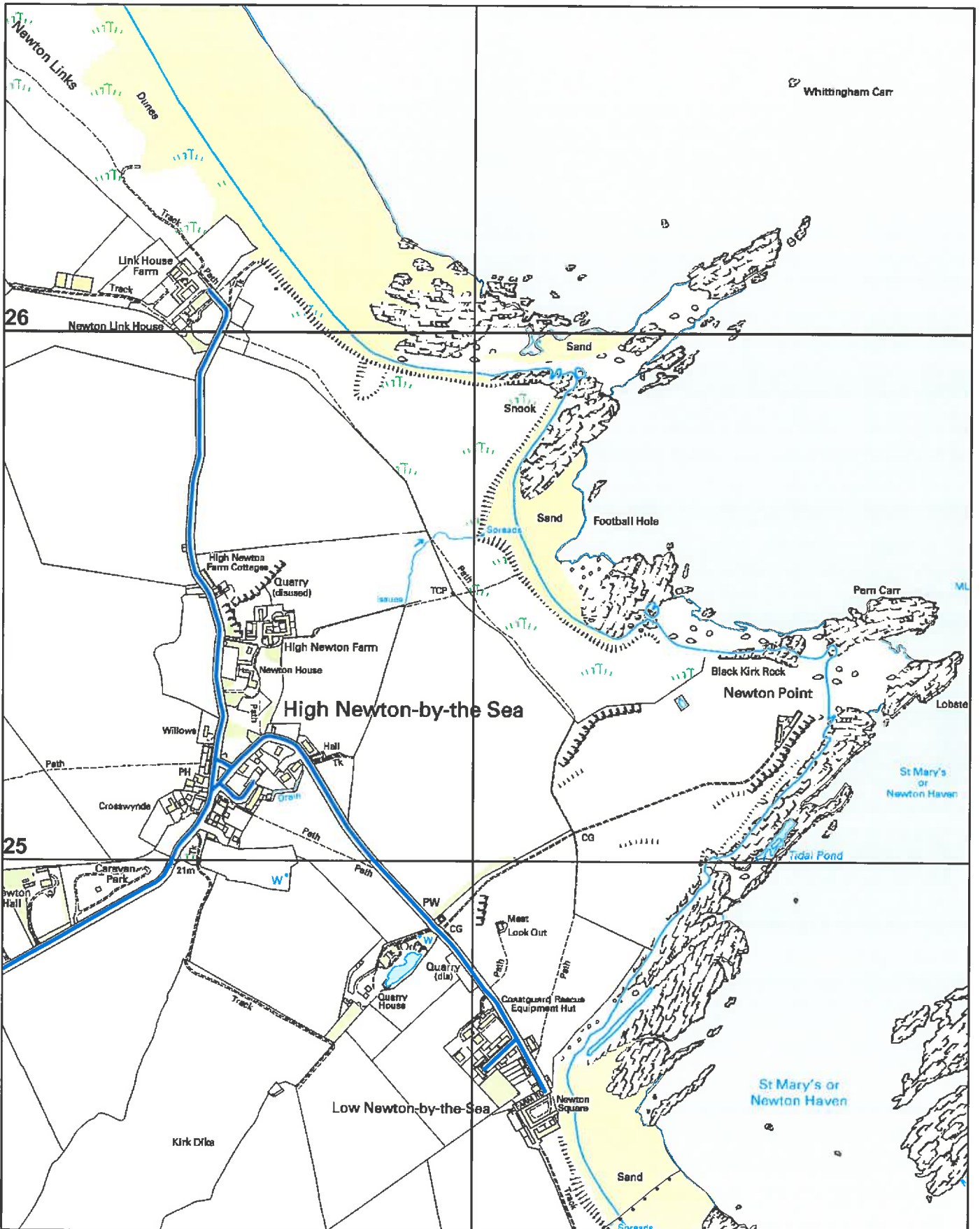
AREA OF LAND COLOURED RED 172 SQ. YDS.
(144 SQ. m.)

Extract from the consolidated Definitive Map
(Relevant Date: 1 Dec 2005)

Beadnell Bay

Robin Wood's Rock





Network Management
Information System

Highways Act 1980 Section 36(6)
County of Northumberland
List of Streets which are highways
maintainable at the public expense
as at 02-May-2006

This map is reproduced from Ordnance Survey material with the permission of Ordnance Survey on behalf of the Controller of Her Majesty's Stationary Office Crown Copyright. Unauthorised reproduction infringes Crown Copyright and may lead to prosecution or civil proceedings. License no. 100049048 (2015).

Drn:	Date:	Scale:
AB	Jan 2019	1:10,000